



www.peachtreecornersga.gov

## COUNCIL MEETING AGENDA

Mike Mason, Mayor

Phil Sadd – Post 1, Council Member  
James Lowe – Post 2, Council Member  
Alex Wright – Post 3, Council Member

Jeanne Aulbach – Post 4, Council Member  
Lorri Christopher – Post 5, Council Member  
Weare Gratwick – Post 6, Council Member

---

April 19, 2016

### COUNCIL AGENDA

7:00 PM

PEACHTREE CORNERS CITY HALL

147 TECHNOLOGY PARKWAY, PEACHTREE CORNERS, GA 30092

---

**A) CALL TO ORDER**

**B) ROLL CALL**

**C) PLEDGE OF ALLEGIANCE** - Colors presented by Boy Scout Troup # 1963.

**D) MAYOR'S OPENING REMARKS**

**E) CONSIDERATION OF MINUTES** – March 15, 2016

**F) CONSIDERATION OF MEETING AGENDA**

**G) PUBLIC COMMENTS**

**H) CONSENT AGENDA – No Items**

**I) PRESENTATIONS AND REPORTS**

1. **Mayor Mason** Proclamation for Tom Rice
2. **Mayor Mason** Proclamation for Earth Day
3. **Diana Wheeler** Staff Activity Report
4. **Greg Ramsey** Staff Activity Report

**J) OLD BUSINESS** - There is no old business.

**K) NEW BUSINESS**

1. **O2016-04-71**  
**D.Wheeler** First Read and Consideration of an Ordinance to amend the City of Peachtree Corners Zoning Map pursuant to RZ2016-003, South Berkeley Lake Rd. Auto Storage, request to rezone from M-1 to M-2 for an auto storage facility on 4.09 acres at 4684 South Berkeley Lake Rd., in District 6, Land Lots 269, Peachtree Corners, GA. (Second read will be on May 17, 2016.)

2. **R2016-04-59  
J. Putnam** Consideration of a Resolution to adopt guidelines for posting material on Social Media platforms and accounts maintained by the City of Peachtree Corners.
3. **R2016-04-60  
D. Wheeler &  
P. Floyd** Consideration of a Resolution to approve the extension of the intergovernmental agreement regarding the Town Center project.
4. **R2016-04-61  
B. Branham** Consideration of a Resolution of The City of Peachtree Corners recognizing Georgia Cities Week, April 17-23, 2016 and Encouraging all citizens to support the celebration and corresponding activities.
5. **Action Item  
G. Ramsey** Street resurfacing contract – FY16 Phase II
6. **Action Item  
G. Ramsey** Emergency street reconstruction contract.
7. **Action Item  
B. Branham** Consideration of funding for the Peachtree Corners Business Incubator, Inc

#### **L) WORK SESSION**

1. **Greg Ramsey** Crosswalk study report – Winters Chapel Road & East Jones Bridge Road
2. **Greg Ramsey** State Route 141 Intersections Analysis and Recommendations – PTC 15.16
3. **Greg Ramsey** Banners on Georgia Power poles
4. **Greg Ramsey** Street lighting – discussion on city lighting (arterials & collectors)
5. **Brandon Branham** Amendment to Ch. 14 Occupation Tax Ordinance
6. **Diana Wheeler** Discussion concerning Mechanicsville School

#### **M) EXECUTIVE SESSION**

#### **N) ADJOURNMENT**

# **Minutes**

**March 15, 2016**



**CITY OF PEACHTREE CORNERS**  
**COUNCIL MEETING MINUTES**  
**MARCH 15, 2016 @ 7:00PM**

The Mayor and Council of the City of Peachtree Corners held a Council Meeting at City Hall, 147 Technology Parkway, Suite 200, Peachtree Corners, GA, 30092. An audible copy of the meeting is available from the City Clerk's office. The following were in attendance:

Mayor	Mike Mason
Council Member	Phil Sadd – Post 1
Council Member	Post 2 - Vacant
Council Member	Alex Wright – Post 3
Council Member	Jeanne Aulbach – Post 4
Council Member	Lorri Christopher – Post 5
Council Member	Weare Gratwick – Post 6
City Manager	Julian Jackson
City Clerk	Kym Chereck
Com. Dev. Director	Diana Wheeler
City Attorney	Bill Riley
City Attorney	Joe Leonard
Public Works Director	Greg Ramsey
Comm. Director	Judy Putnam
Finance Director	Brandon Branham

Council Member Sadd attended via telephone conference call, but was not permitted to vote.

**PLEDGE OF ALLEGIANCE:** Mayor Mason led the Pledge of Allegiance.

**MINUTES:**

**MOTION TO APPROVE THE MINUTES FROM THE FEBRUARY 16, 2016 COUNCIL MEETING.**

2016-03-15

Council Meeting Minutes

Page 1 of 14

**By: Council Member Christopher**  
**Seconded by: Council Member Gratwick**  
**Vote: (5-0) (Christopher, Gratwick, Mason, Wright, Aulbach)**

**MOTION TO APPROVE THE MINUTES FROM THE FEBRUARY 28, 2016 SPECIAL CALLED COUNCIL MEETING.**

**By: Council Member Christopher**  
**Seconded by: Council Member Gratwick**  
**Vote: (5-0) (Christopher, Gratwick, Mason, Wright, Aulbach)**

**PUBLIC COMMENT:** There was no public comment.

**CONSENT AGENDA:**

**APH 2015-09-029**

Consideration of Approval of an Alcoholic Beverage License Application for Consumption on Premise, Wine & Malt Beverage License for Pub Ten Inc. at 5270 Peachtree Pkwy, Ste 118B, Peachtree Corners, GA 30092.

**MOTION TO APPROVE APH 2015-09-29.**

**By: Council Member Wright**  
**Seconded: Council Member Christopher**  
**Vote: (5-0) (Wright, Christopher, Mason, Aulbach, Gratwick)**

**APH 2015-09-030**

Consideration of Approval of an Alcoholic Beverage License Application for Consumption on Premise, Wine & Malt Beverage License for PK International Food Merchant Inc DBA: Royal Bistro at 6365 Spalding Dr, Ste A, Peachtree Corners, GA 30092.

**MOTION TO APPROVE APH 2015-09-30.**

**By: Council Member Wright**  
**Seconded: Council Member Christopher**  
**Vote: (5-0) (Wright, Christopher, Mason, Aulbach, Gratwick)**

**APH 2015-09-031**

Consideration of Approval of an Alcoholic Beverage License Application for Retail/Package, Wine & Malt Beverage License for Express Point 2, LLC DBA: Express Food Mart at 3426 Holcomb Bridge Road, Peachtree Corners, GA 30092.

**MOTION TO APPROVE APH 2015-09-31.**

**By: Council Member Wright**

**Seconded: Council Member Christopher**

**Vote: (5-0) (Wright, Christopher, Mason, Aulbach, Gratwick)**

**PRESENTATIONS AND REPORTS:**

**P2016-03-12 Proclamation for James Lowe**

Mayor Mason presented a proclamation to James Lowe. Mr. Lowe was the first Council Member for Post two, and resigned to seek election for State Representative. The Mayor recognized Mr. Lowe's accomplishments and thanked him for his contributions.

**P2016-03-11 Proclamation for Arbor Day**

Mayor Mason presented a proclamation declaring March 19, 2016 as Arbor Day. Mayor Mason invited everyone to the Arbor Day Celebration on Saturday, March 19, 2016 at 11:00 AM, at Simpsonwood Park.

**Staff Activity Report – Community Development**

Mrs. Diana Wheeler, Community Development Director, provided her report on staff activities that occurred during the period of February 22, 2016 – March 11, 2016. These activities included, among other items, meetings with a consultant to discuss the Multi-Family Housing Redevelopment Strategies Study, meeting with the Attorney to discuss the Town Center contract extension, meeting with the Festival Committee to discuss booth setup, and preparing a preliminary department budget.

**Staff Activity Report – Public Works**

Mr. Greg Ramsey, Public Works Director, provided his report on staff activities that occurred in the period ending with March 15, 2016. These activities included, among other items, attending a meeting for the Peachtree Parkway sewer extension, attending a meeting with the Bridge Committee, attending a meeting concerning pedestrian crossings with GCDOT, and attending a GCDOT Comprehensive Transportation Plan Stakeholder Meeting.

**OLD BUSINESS:**

**O2016-02-66**

Second Read and Consideration of an Ordinance to amend the City of

Peachtree Corners Zoning Map pursuant to RZ2016-001/ SUP2016-001, South Old Peachtree Rd. Auto Repair, request to rezone and approve a special use permit for an auto repair and storage facility on 6.89 acres at 4285 South Old Peachtree Rd., in District 6, Land Lots 259 and 268, Parcels 016, 373 and 374, Peachtree Corners, GA.

**MOTION TO APPROVE O2015-02-66 WITH 14 CONDITIONS.**

**By: Council Member Aulbach**

**Seconded: Council Member Gratwick**

**Vote: (5-0) (Aulbach, Gratwick, Mason, Wright, Christopher)**

*Approved Conditions for O2015-02-66*

- 1) *Vehicle storage shall not be located within the front yard.*
- 2) *Vehicle storage shall be screened by a solid wood fence, masonry wall, or slatted chain-link fence at least eight (8) feet high.*
- 3) *Vehicle storage shall be limited to one automobile per parking space. All vehicles must be parked in designated spaces and no 'tandem' or 'valet' parking shall be permitted. A parking plan shall be submitted to Staff for approval.*
- 4) *No inoperable (junk/salvage) vehicles shall be stored outdoors.*
- 5) *No vehicles shall be located on unpaved surfaces.*
- 6) *Vehicles or materials stored outdoors shall not be placed or stacked at a height exceeding that of the screening fence.*
- 7) *All vehicle maintenance and mechanical work shall be conducted within an enclosed building.*
- 8) *The existing magnolia trees at the front of the property shall be preserved and no new parking spaces shall be constructed between the existing office structure and South Old Peachtree Road.*
- 9) *No billboards shall be permitted on the property.*
- 10) *Dumpsters shall not be located in front of the existing office structure and shall be screened by an opaque decorative wall, at least six (6) feet in height, which will coordinate with the building architecture.*

- 11) *Outdoor lighting shall be contained in cutoff-type luminaries and shall be directed inward toward the property so as not to reflect into adjacent properties or to create a hazard for passing automobile traffic.*
- 12) *Permanent freestanding project signage shall be limited to one monument sign to be located at the front of the property.*
- 13) *Any outside speakers shall not be audible from adjacent properties.*
- 14) *All conditions must be met before a business license can be issued.*

**O2016-02-67**

Second Read and Consideration of an Ordinance to amend the City of Peachtree Corners Zoning Map pursuant to RZ2015-006 / SUP 2015-005/ V2016-002 Twin Lakes, Request to rezone ten parcels in District 6 of Peachtree Corners, Georgia, to allow commercial, multi-family, and trail development and approve a special use permit for a liquor store and associated variances on a total of 38.73 acres, as follows:

- Parcel #1 Rezone from M-1, Light Industry, to TO, Trails and Open Space, an 8.87 acre tract located in LL284, parcel 015 at 3550 Engineering Dr.
- Parcel #2 Rezone from M-1, Light Industry, to C2, Commercial, a 1.9 acre tract LL284, parcel 016 and 018 on Peachtree Parkway at Engineering Dr.
- Parcel #3 Rezone from M-1, Light Industry, to C2, Commercial, and approve a special use permit for a liquor store on a 4.7 acre tract located in LL284, parcel 018 on Peachtree Parkway at Engineering Dr.
- Parcel #4 Rezone from M-1, Light Industry, to C2, Commercial, and TO, Trails and Open Space, a 1.23 acre tract located in LL 285, parcel 056 on Technology Parkway near Westech Dr.
- Parcel #5 Rezone from M-1, Light Industry, to TO, Trails and Open Space, a .73 acre tract located in LL284, parcel 040 on Technology Pkwy. near Westech Dr.
- Parcel #6 Rezone from M-1, Light Industry, to TO, Trails and Open Space, a .35 acre tract located in LL284, parcel 041 on Technology Pkwy. near Westech Dr.
- Parcel #7 Rezone from M-1, Light Industry, to TO, Trails and Open Space, a 4.33 acre tract located in LL285, parcel 010 on Technology Pkwy. near Westech Dr.
- Parcels #8, #9, #10 Rezone from M-1, Light Industry, to RM-13, Multi-Family Residence, three tracts consisting of 14.03 acres located in LL272, parcels 061, 062, and 064 on Peachtree Industrial Blvd. and 510 and 520 Guthridge Ct.

## DRAFT COPY

**MOTION TO APPROVE THE ORDINANCE PROVIDED WITH “RED LINES” WITH THE ADDITION OF COUNCIL MEMBER AULBACH’S CONDITION NUMBER 30, WHICH STATES THAT THE APPLICANT WILL JOIN THE MULTI-FAMILY CRIME FREE PREVENTION PROGRAM; AND, ADDING IN CONJUNCTION WITH THE TRANSFERS, THE PROPERTY OWNER SHALL CAUSE \$41,250.00 TO BE CONTRIBUTED TO AN ACCOUNT DESIGNATED BY THE CITY OF PEACHTREE CORNERS (AS STATED IN THE CONDITIONS BELOW).**

**By: Council Member Christopher**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Aulbach, Gratwick, Mason, Wright, Christopher)**

*Approved Conditions for O2015-02-67*

*With regard to Parcels #1, 6, and 7 as shown on ‘Attachment A’, Property Zoning Map, dated 1/2/16:*

- 1. These properties shall be rezoned T-O, Trails and Open Space*
- 2. Prior to the issuance of the first Certificate of Occupancy on Parcels zoned C-2 or RM-13, Parcels #1, 6 and 7 shall be deeded to the City of Peachtree Corners Downtown Development Authority. In conjunction with the transfers, the property owner shall cause \$41,250.00 to be contributed to an account designated by the City of Peachtree Corners which funds shall be used by the City (or its designee) to construct a multi-use trail (and/or related infrastructure such as a lake dock) between the termination of the Engineering Drive extension and Technology Park lake (within Parcel 7).*
- 3. Pursuant to Ordinance 2015-11-59, Trails and Open Space, the 13.55 acres attributed to the T-O zoned parcels shall be assigned 13 multi-family density unit credits per acre for a total of 176 density units. 113 of the 176 total density units shall be allocated to the multi-family residential development shown on Parcels #8, #9, and #10. The remaining 63 surplus density units shall be credited to an account established for the property owner.*

*With regard to Parcels #2, 3, 4 and 5 as shown on ‘Attachment A’, Property Zoning Map, dated 1/2/16:*

- 4. The properties shall be rezoned C-2, commercial and shall be developed in general conformity with the submitted site plan prepared by Planners and Engineers Collaborative dated 11/15/15 (except as noted in condition #11).*
- 5. A Special Use Permit shall be approved on Parcel #3 for a Liquor Store use.*

## DRAFT COPY

6. *The property owner or subsequent developer shall construct an 8' wide paved path along the Engineering Drive extension. The paved path shall be completed prior to the issuance of the first Certificate of Occupancy for the first building constructed on the C-2 zoned parcels.*
- 6.5. *For the portion of the property which is developed to accommodate the commercial pads, the landscape strip adjoining the paved path shall be planted with hardwood trees which are a minimum of 10 feet tall at the time of planting and are spaced on 25 foot centers in order to create a boulevard effect along Engineering Drive. The entire C-2 development shall have a cohesive landscape plan which shall be approved by staff.*
7. *If the City's Multi-Use Trail winds around back of the convenience store, the right-in only driveway shall be designed to accommodate a trail crossing.*
8. *The property owner or subsequent developer shall be responsible for all traffic and roadway improvements required to construct a new intersection at Engineering Drive and Peachtree Parkway and to extend Engineering Drive to Technology Parkway. These improvements shall include, but not be limited to: traffic light, crosswalks, the paved path referenced in Condition 6, bike lane, and, where necessary, deceleration lanes.*
9. *Development parcels shall be designed to mitigate the amount of impervious surface associated therewith as is reasonably possible in order to retain as much of the existing topography, trees, and stream as reasonably practicable. Retaining walls shall be built to minimize the footprint of the commercial buildings and parking areas. Setback requirements may be reduced if doing so benefits the preservation of trees or stream. However, the landscape strip along Peachtree Parkway required by Overlay Design Standards (and as modified by Condition 10) shall be provided.*
10. *Prior to an issuance of the Certificate of Occupancy for each building, an enhanced landscape strip shall be planted along the entire frontage of the property frontage along Peachtree Parkway. This enhanced landscape strip shall consist of, at a minimum, double staggered rows of evergreen plants, to be at least 3 feet at maturity, sufficient to prevent, within one year of planting, the view of all pavement surrounding the gas pumps and buildings by passengers in vehicles traveling along Peachtree Parkway. Said landscape strip shall be approved by the Community Development Director and maintained for this purpose at all times by the property owner.*
11. *To retain as many of the natural features at the rear of the properties, surface parking spaces shall be located to the front and sides of the buildings.*

## DRAFT COPY

12. *Architectural elevations for the gas station and liquor store shall be substantially similar to the drawings submitted via letter and dated 1/29/16. The gas pump canopy columns shall be clad in masonry and the design and material approved by the Director of Community Development.*
13. *Stream setback requirements shall be waived in order to accommodate the developments on C-2 zoned properties. Piping of the stream shall be permitted provided that only the least amount of piping that can be demonstrated to be reasonably necessary is installed.*
14. *The property owner or subsequent developer shall be responsible for median landscaping (installation and maintenance) in Peachtree Parkway for the length of the median contiguous to the application property. Landscaping plans shall be subject to Staff's approval.*
15. *Sanitary Sewer line relocation shall be accomplished in the manner that will have the least environmental impact to the site.*
16. *Detention ponds visible from roadways shall be screened with landscape plantings to be approved by Staff.*

*With regard to Parcels #8, #9, and #10 as shown on 'Attachment A', Property Zoning Map, dated 1/2/16:*

17. *The property owner or subsequent developer shall be responsible for all traffic and roadway improvements required at Peachtree Industrial Boulevard and Guthridge Ct. and along Guthridge Ct in connection with the development of the subject property for millennial housing.*
18. *The property owner or subsequent developer shall make every reasonable effort to acquire a pedestrian / bicycle access easement between the Guthridge Ct. residential development and Technology Parkway South. The residential development shall include a dedicated pedestrian / bicycle access at the closest point of the easement.*
19. *Millennial housing development shall not exceed 295 units. The units shall be limited to one and two bedroom floor plans. Two bedroom plans shall be limited to a maximum of 30% of the units.*
20. *Site development shall be substantially similar to plans prepared by Planners and Engineers Collaborative dated 11/18/15 except that buildings located along Guthridge Ct. and Peachtree Industrial Boulevard shall maintain a 10 ft. setback from the property line and that 10 ft. strip shall be landscaped with plant material approved by Staff.*

## DRAFT COPY

21. *The owner or subsequent developer shall landscape and maintain the right-of-way immediately in front of the millennial housing project along Peachtree Industrial Boulevard.*
22. *Site amenities shall include: pedestrian / bike trail; fitness facility, a dog park, an electric car recharge station, and a lake dock; No children's playground equipment shall be permitted.*
23. *A central mail kiosk and a trash and recycling station shall be designed to match the main buildings and shall be located near Guthridge Ct.*
24. *Every reasonable effort shall be made to preserve specimen trees near the lakes.*
25. *Building designs shall incorporate features that are compliant with Peachtree Corners' Green Building Ordinance wherever practical.*
26. *Interior features for residential units shall be in general conformity with the pictures presented to the Planning Commission on February 9, 2016 and include:*
  - *Stainless appliances*
  - *Gourmet kitchens with granite countertops*
  - *Designer ceramic tile back splashes*
  - *Wood cabinets*
  - *Upscale plank or engineered wood flooring*
  - *Designer lighting*
  - *Granite countertops and ceramic tile tub surrounds in bath*
  - *Nine foot ceilings*
  - *Ceiling fans in primary living space*
  - *Generous closets*
  - *Connections for full size, stackable washer/dryer units*
  - *Walkout balconies*
27. *Community features for millennial housing shall be in general conformity with the pictures presented to the Planning Commission on February 9, 2016 and include:*
  - *Resort-style pool and courtyard with tanning deck*
  - *Dock with aquatic amenities (i.e. paddle boards and kayaks)*
  - *Expansive clubhouse with cyber café / wi-fi and business center*
  - *Outdoor grilling and entertainment space with fireplace and bocce ball court*
  - *State-of-the-art fitness center*
28. *Prior to the issuance of the first Certificate of Occupancy on Parcels zoned RM-13, the property owner shall deed a 25' wide strip along the subject*

## DRAFT COPY

*property's western boundary and the area of the property beginning 25' from the top of the lake bank and extending to the property line within the lake to the City of Peachtree Corners Downtown Development Authority. Prior to such transfer, the property owner and/or developer shall install a 12' wide multi-use trail and lake dock within said area.*

29. *If there are any material changes to the site plans and/or architectural elevations, as applicable, for the C-2 and/or RM-13 components of the development, then the property owner and/or developer shall submit such revised site and architectural plans to the Planning Commission for approval.*
30. *The millennial housing development shall participate in the crime-free multi-family housing program.*

### **O2016-02-68**

Second Read and Consideration of an Ordinance to amend the City of Peachtree Corners Zoning Map pursuant to V2016-001, Town Center, Request for a stream buffer variance (in conjunction with SUP2015-003 approved 6/9/2015) for a portion of a 19.6 acre property located at Peachtree Parkway near Forum Dr. in District 6, Land Lot 301, Parcel 183, Peachtree Corners, GA.

### **MOTION TO APPROVE O2015-02-68.**

**By: Council Member Gratwick**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Gratwick, Aulbach, Mason, Wright, Christopher)**

*Approved Conditions for O2015-02-68.*

- 1) *An encroachment into the 50 ft. undisturbed buffer and 75 ft. impervious stream setback line shall be permitted for the improvements planned along a portion of the southern property line as shown on the submitted Hanes Gibson and Associates drawings dated 1-22-16.*
- 2) *The face of the Gabion retaining wall shall be vegetated as shown on the submitted Hanes Gipson and Associates drawings dated 1-22-16.*

### **O2016-01-64**

Second Read and Consideration of an Ordinance to amend the City of Peachtree Corners Zoning Resolution Article XII, pursuant to PH2015-008 by amending Sec. 1310, M-1, Light Industry District, in order to limit permitted uses within the Central Business District.

**MOTION TO APPROVE O2015-02-64.**

**By: Council Member Gratwick**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Gratwick, Aulbach, Mason, Wright, Christopher)**

**O2016-02-65**

Second Read and Consideration of an Ordinance to amend the Code of Ordinances of the City of Peachtree Corners, Georgia, Article II, Sec. 42-24, Noise, to limit the hours of operation for lawn maintenance equipment and trash hauling vehicles, and extend construction hours.

**MOTION TO APPROVE O2015-02-65.**

**By: Council Member Aulbach**

**Seconded: Council Member Christopher**

**Vote: (5-0) (Aulbach, Christopher, Mason, Wright, Gratwick)**

**NEW BUSINESS:**

**R2016-03-58**

Consideration of a Resolution to Approve and Participate in the Georgia Municipal Association Defined Compensation Plan for the City of Peachtree Corners and for Other Purposes.

**MOTION TO APPROVE R2016-03-58.**

**By: Council Member Gratwick**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Gratwick, Aulbach, Mason, Wright, Christopher)**

**R2016-02-56**

Consideration of a Resolution for a call of Election, to announce qualifying dates and fees, and to appoint Election Officials.

**MOTION TO APPROVE R2016-02-56.**

**By: Council Member Wright**

**Seconded: Council Member Christopher**

**Vote: (5-0) (Wright, Christopher, Mason, Aulbach, Gratwick)**

**ACTION ITEM**

Consideration of Awarding a Multi-Family Housing Redevelopment Study for the Holcomb Bridge Road Corridor to Bleakley Advisory Group.

**MOTION TO AWARD A MULTI-FAMILY HOUSING REDEVELOPMENT STUDY FOR THE HOLCOMB BRIDGE ROAD CORRIDOR TO BLEAKLY ADVISORY GROUP.**

**By: Council Member Wright**

**Seconded: Council Member Christopher**

**Vote: (5-0) (Wright, Christopher, Mason, Aulbach, Gratwick)**

**ACTION ITEM**

Consideration of an Intergovernmental Agreement between the City of Peachtree Corners and the Gwinnett County Board of Registrations and Elections for City Elections Using Election Equipment.

**MOTION TO APPROVE AN INTERGOVERNMENTAL AGREEMENT BETWEEN THE CITY OF PEACHTREE CORNERS AND THE GWINNETT COUNTY BOARD OF REGISTRATIONS AND ELECTIONS FOR CITY ELECTIONS USING ELECTION EQUIPMENT.**

**By: Council Member Gratwick**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Gratwick, Aulbach, Mason, Wright, Christopher)**

**ACTION ITEM**

Consideration of Awarding a Task Order from the On Call Consulting Contract for a Comprehensive Transportation Plan.

**MOTION TO AWARD POND AND COMPANY THE CONTRACT FOR A COMPREHENSIVE TRANSPORTATION PLAN.**

**By: Council Member Christopher**

**Seconded: Council Member Gratwick**

**Vote: (5-0) (Christopher, Gratwick, Mason, Wright, Aulbach)**

**ACTION ITEM**

Consideration of Awarding a Construction Contract for installation of landscaping in the Peachtree Parkway Median from Medlock Bridge Road to the Chattahoochee River.

**MOTION TO AWARD A CONSTRUCTION CONTRACT TO RUSSELL LANDSCAPING FOR INSTALLATION OF LANDSCAPING IN THE PEACHTREE PARKWAY MEDIAN FROM MEDLOCK BRIDGE ROAD TO THE CHATTAHOOCHEE RIVER.**

**By: Council Member Gratwick**

**Seconded: Council Member Aulbach**

**Vote: (5-0) (Gratwick, Aulbach, Mason, Wright, Christopher)**

**ACTION ITEM**

Nomination and Election of Mayor Pro tem.

Council member Wright nominated Council Member Gratwick for Mayor Pro tem.

**VOTE FOR COUNCIL MEMBER GRATWICK AS MAYOR PRO TEM.**

**Vote: 5-0 (Wright, Mason, Aulbach, Christopher, Wright)**

**WORK SESSION:**

**Discussion concerning Community Theater.**

Council Member Wright informed the Council that he requested Staff to conduct preliminary research into the components and logistics of a community theater. After discussion it was determined that the City would form an Arts Council. It was requested that the Mayor and each Council member select two members to be appointed to the Arts Council, and for Staff to search for a professional consultant.

**Discussion on Traffic Study for SR 141/Peachtree Parkway intersections.**

Mr. Greg Ramsey, Public Works Director, informed the Mayor and Council that the ARC will be starting their traffic study, which is managed by GDOT, for State Road 141 and Peachtree Parkway intersections.

**Update on Town Center Financing**

Mr. Julian Jackson, City Manager, informed the Mayor and Council that the financing for the town center is due on May 13, and that Ameris Bank has agreed to extend the financing for another year at the at the same interest rate.

**Discussion on Posting Guidelines for Social Media and Community Calendar.**

Mrs. Judy Putnam, Communications Director, presented the Mayor and Council posting guidelines for the Community Calendar and Social Media. After discussion it was determined that this item would come before the Mayor and Council at the next meeting in the form of a Resolution.

**Update on Multi-Family Housing**

Mr. Brandon Branham, Finance Director, gave a brief update of the apartment inspections. Mr. Branham stated that the inspections will be completed in May 2016 and he will give a final report in June 2016.

**Update on GIS database**

Mr. Brandon Branham, Finance Director, gave a presentation on our current GIS database and gave an option for an advanced, user friendly GIS database. It was determined that a RFQ would be released for a GIS user interface.

**EXECUTIVE SESSION:** There was no Executive Session.

**ADJOURNMENT:**

**MOTION TO ADJOURN AT 9:54 PM.**

**By: Council Member Gratwick**

**Seconded by: Council Member Christopher**

**Vote: (5-0) (Gratwick, Christopher, Mason, Wright, Aulbach)**

Approved,

Attest:

\_\_\_\_\_  
Mike Mason, Mayor

\_\_\_\_\_  
Kymberly Chereck, City Clerk  
(Seal)

**Proclamation**

**Tom Rice**

# *Proclamation*

**A PROCLAMATION OF THE CITY OF PEACHTREE CORNERS, GEORGIA  
THANKING GEORGIA STATE REPRESENTATIVE TOM RICE FOR HIS  
DEDICATED SERVICE**

WHEREAS, State Representative Tom Rice served district 95 with dedication, purpose, and commitment for 20 years; and

WHEREAS, during his tenure, Representative Rice promoted numerous initiatives that improved quality of life in all of Georgia and in his home district including regulations governing motor vehicle safety and protection of commerce and trade; and

WHEREAS, In addition to all of his other initiatives, Representative Rice's successful efforts ultimately led to the creation of Gwinnett County's newest and largest municipality, the City of Peachtree Corners; and,

WHEREAS, despite the urging of his constituents to pursue another term, Representative Rice has instead chosen to retire and pursue a more leisurely agenda;

NOW, THEREFORE, BE IT PROCLAIMED by the Mayor and Council of the City of Peachtree Corners that the City thanks Tom Rice for his dedicated service and his numerous contributions that have enhanced the quality of life in Georgia and in the City of Peachtree Corners. It is further proclaimed that Tom Rice's work on Peachtree Corners' behalf will always be recognized as a valuable and integral part of the city's history.

SO PROCLAIMED AND EFFECTIVE, this the 19<sup>th</sup> day of April, 2016.

Attest:

Approved:

\_\_\_\_\_  
Kym Chereck, City Clerk

\_\_\_\_\_  
Mike Mason, Mayor

# **Proclamation**

## **Earth Day**

# *Proclamation*

**A PROCLAMATION OF THE CITY OF PEACHTREE CORNERS, GEORGIA  
SUPPORTING EARTH DAY, APRIL 22, 2016, AND ENCOURAGING ALL  
CITIZENS TO CELEBRATE AND PROTECT THE ENVIRONMENT AND  
CONTRIBUTE TO A SUSTAINABLE COMMUNITY**

WHEREAS, the global community now faces extraordinary challenges, including global health issues, food and water shortages, and economic struggles; and

WHEREAS, all people, regardless of race, gender, income, or geography, have a moral right to a healthy, sustainable environment with economic growth; and

WHEREAS, the citizens of the global community must step forward and take action to create a green economy to combat these global challenges; and

WHEREAS, it is acknowledged that even global initiatives must first be started at the local level in order to effect true change and achieve success;

NOW, THEREFORE, BE IT PROCLAIMED by the Mayor and Council that the City of Peachtree Corners supports Earth Day, April 22, 2016, and encourages all citizens to celebrate and protect the environment and contribute to the sustainability of the City of Peachtree Corners.

SO PROCLAIMED AND EFFECTIVE, this 19th day of April, 2016.

Attest:

Approved:

\_\_\_\_\_  
Kym Chereck, City Clerk

\_\_\_\_\_  
Mike Mason, Mayor

**Staff Activity  
Report  
D. Wheeler**



# Memo

TO: Mayor and Council

CC: Julian Jackson, City Manager

FROM: Diana Wheeler, Community Development Director

DATE: April 15, 2016

SUBJECT: Staff Activity Report

The following is a summary of Staff activity during the period of 3/28/16 – 4/15/16.

- A. Meetings with:
  1. Citizens regarding multi-use trail ideas including bird watching and disc golf
  2. Attorney to discuss Town Center contract extension
  3. High Schools to setup bridge information for student input
- B. Worked with Landmarks Committee and Cherryllion Studio on concept for Founders Monument.
- C. Conducted search for art consultant.
- D. Developing an Economic Development packet.
- E. The following permits applications were received:

DATE	Permit #	NAME	ADDRESS	TYPE
PP16-0283		ATLANTIS PLUMBING	3982 JONES BRIDGE CIRCLE	PLUMBING
PP16-0284		TIDEWATER BUILDER LLC	3375 HOLCOMB BRIDGE RD STE F	DEMO
PP16-0285		MALON D MIMMS CO	6275 SPALDING DRIVE STE E-1	HVAC
PP16-0286		ATLANTA DECKING AND FENCE CO., INC	6060 NEELY FARM DR	ADDITION
PP16-0287		KIDSPLOSION	7085 JIMMY CARTER BLVD STE B & C	INTERIOR FINISH
PP16-0288		PEACHTREE SERVICE EXPERTS LLC	5248 GARNABY LN	HVAC
PP16-0289		PEACHTREE SERVICE EXPERTS LLC	5680 SPUR CIRCLE	HVAC
PP16-0290		SOUTHERN PROERTY RESTORATION	5100 S OLD PEACHTREE RD	INTERIOR FINISH
PP16-0291		SENOIA ELECTRIC	3655 WESTCHASE VILLAGE LN 3830 A-1	ELECTRICAL
PP16-0292		SENOIA ELECTRIC	3655 WESTCHASE VILLAGE LN 3850 E-2	ELECTRICAL
PP16-0293		STORM GUARD EXTERIOR RESTORATION	3765 SPALDING PARK DR	RE-ROOF
PP16-0294		PATRICK & KAREN FOLEY	5116 STAVERLY LN	DECK ADDITION
PP16-0295		BENCO CONSTRUCTION INC	4277 STILSON CIRCLE	REPAIR
PP16-0296		DANIEL'S PLUMBING	3655 WESTCHASE VILLAGE LN	PLUMBING
PP16-0297		NATIONAL CUSTOM CORPORATE SERVICES INC	5430 METRIC PLACE	INTERIOR FINISH
PP16-0298		RELIABLE HEATING&AIR,RH&A LLC	4995 RIVERFIELD DR	HVAC
PP16-0299		STRADIS MEDICAL	3025 NORTHWOODS PKWY	RACKING
PP16-0300		FIX-R-US	3903 CRAB ORCHARD LN	PLUMBING
PP16-0301		THD @ HOME SERVICES	5025 FELHURST WAY #2-B	REPAIR/REPLACE
PP16-0302		BILL STARK PROPERTIES	4378 RIVERVIEW DRIVE	DEMO
PP16-0303		TEDRRY WILLIAMS ROOFING	4043 EVERETT CT	RE-ROOF
PP16-0304		HAND CAR WASH	3425 MEDLOCK BRIDGE RD STE 400-A	CERTIFICATE OF OCCUPANCY
PP16-0305		SUPERIOR PLUMBING SERVICES	4330 MISSENDALL LN	PLUMBING
PP16-0306		MICHAEL D WOOD	4178 ORCHARD KNOLL	REMODEL
PP16-0307		DECK BUILDERS USA/INSPECTORS	5040 RIVERTHUR PL	DECK
PP16-0308		IMMUCOR	2990 GATEWAY DR	RENOVATIONS
PP16-0309		ALL-TECH HEATING & COOLING	5388 GOLDEN LEAF TRL	HVAC
PP16-0310		ATLANTA DECKING AND FENCE CO., INC	4408 ALLENHURST DR	PORCH ADDITION
PP16-0311		EXOATIONS OF ATLANTA	5861 OSAGE COURT	DECK ADDITION
PP16-0312		LEHMAN CUSTOM POOLS	5115 RIVERLAKE DRIVE	SPA

DATE	Permit #	NAME	ADDRESS	TYPE
PP16-0313		DEAN LE	4196 NEELY MEADOWS CT	DECK ADDITION
PP16-0314		LEWIS REEVES PROPERTIES	4348 RIVERVIEW DR	NEW HOME
PP16-0315		GEORGIA CERTIFIED LLC	6135 PEACHTREE PKWY/ALDI	PERMANENT SIGN
PP16-0316		GEORGIA CERTIFIED LLC	6135 PEACHTREE PKWY /ALDI	PERMANENT SIGN
PP16-0317		G SMITH BUILDERS	3091 GOVERNORS LAKE DR STE 420	INTERIOR FINISH
PP16-0318		ACS, INC	5068 BRIDGEPORT WAY	HVAC
PP16-0319		CULTURALINK	157 TECHNOLOGY PKWY STE 600	INTERIOR FINISH
PP16-0320		SHUMATE MECHANICAL	5023 WILLIAMSPORT DR	HVAC
PP16-0321		CAPITAL ELECTRI CO., INC	45 TECHNOLOGY S PKWY	ELECTRICAL
PP16-0322		CAPITAL ELECTRI CO., INC	157 TECHNOLOGY PKWY	ELECTRICAL
PP16-0323		CAPITAL ELECTRI CO., INC	302 RESEARCH DR	ELECTRICAL
PP16-0324		FASTSIGNS NORCROSS	5185 PEACHTREE PKWY STE 106	PERMANENT SIGN
PP16-0325		BARRON'S HVAC LLC	5525 SAPELO TRL	HVAC
PP16-0326		MANN NOYLE & ASSOCIATES	5270 PEACHTREE PKWY STE 118B	ELECTRICAL
PP16-0327		SHARPE CONTRACTORS	4941 SOUTH OLD PEACHTREE RD	CONSTRUCTION TRLR
PP16-0328		SIGNS & GRAPHICS, INC	4279 BUFORD HWY	PERMANENT SIGN
PP16-0329		SIGNS & GRAPHICS, INC	4279 BUFORD HWY	PERMANENT SIGN
PP16-0330		INSPECTOR DRAIN DBA ROOTER PLUS	5270 PEACHTREE PKWY STE 118B	PLUMBING
PP16-0331		JERAIR M & E SERVICES, INC	4825 BUFORD HWY	HVAC
PP16-0332		SUPERIOR PLUMBING SERVICES	3995 ROYAL PENNON CT	PLUMBING
PP16-0333		SHARPE CONTRACTORS	4941 S OLD PEACHTREE RD	CONSTRUCTION SIGN
PP16-0334		MERIT CONSTRUCTION COMPANY	5555 TRIANGLE PKWY	INTERIOR FINISH

### Code Enforcement Summary – March 2016

New Cases	<u>142</u>	
NOV's issued	<u>73</u>	
Citations issued	<u>0</u>	
Signs Removed from ROW	<u>47</u>	(approximate)
Citizen Complaints	<u>68</u>	
Field Generated Cases:	<u>74</u>	
Cases by Type:		

Residential: 60

Commercial: 82

#### Violations by Type

Property Maintenance	<u>19</u>	Parking Illegally	<u>1</u>
Trash	<u>27</u>	Open Storage	<u>2</u>
RV/ Non-motor vehicle	<u>0</u>	Junk Vehicle	<u>4</u>
High Grass/Weeds	<u>2</u>	Other (Code Enforcement)	<u>4</u>
Illegal Signs	<u>2</u>	Other (Property Violation)	<u>69</u>
No Business License	<u>0</u>	Trees	3 <u>2</u>
Building w/o Permit	<u>2</u>	Animated Signs	0 <u>0</u>
	<u>1</u>		

#### Year-To-Date as of 2/29/2016

2016 Cases	<u>336</u>
2016 NOV's issued	<u>220</u>
2016 Citations issued	<u>0</u>
2016 Signs Removed from ROW	<u>182</u>
2016 cases closed with court action:	<u>0</u>

**Staff Activity  
Report  
G. Ramsey**



## MEMO

---

TO: Mayor & Council  
CC: Julian Jackson, City Manager  
FROM: Greg Ramsey, P.E., Public Works Director  
DATE: April 19, 2016  
SUBJECT: Public Works Activity Report

---

The following is a summary of the Public Works Activities in the monthly period ending 04-11-16:

A. Attended the following meetings:

1. Spalding Road widening concept meeting with Gwinnett & Sandy Springs, 4-8-16
2. Cross walk studies meeting, 4-7-16
3. SR 141 traffic study meeting, 4-7-16
4. SR 141 Median Landscaping kickoff, 4-7-16
5. Twin Lakes development, 3-31-16
6. Peachtree Marketplace Phase III, 3-31-16
7. MS4 inspections meeting with DWR, 3-29-16
8. FEMA reimbursement workshop, 3-23-16
9. Kickoff Meeting, Comp Trans Plan, 3-22-16
10. Gwinnett DOT, Jimmy Carter at Holcomb Bridge, 3-18-16
11. Gwinnett Stormwater Committee, 3-18-16

B. Field Services Operations 03-08-16 thru 04-11-16

1. # of Work Orders Initiated = 136
2. # of Fix It App submittals for PW = 13
3. # of Field Generated Work Orders = 123
4. # of Work Orders Completed = 124
5. # of Work Orders Referred to Other Departments = 9
6. Please see below for summaries of Work Orders & Fix-It App submittals

C. Capital Improvement Project updates

1. 15.06 Peachtree Parkway widening at Peachtree Industrial Blvd, construction letting scheduled for April 2016 by Gwinnett DOT
2. 15.01 Winters Chapel Road sidewalks – project at substantial completion, holding retainage pending growth of seeding and final stabilization
3. 15.05 Technology Parkway sidewalks – construction underway, making good progress
4. 15.03 Holcomb Bridge Road at Jimmy Carter Blvd – survey completed in December, design and traffic analysis underway now, design completion scheduled for end of February
5. 15.08 SR 141 traffic intersection analysis – kicked off in December, nearly 50% complete on analysis

6. 15.11 Roundabout and pedestrian improvements on Peachtree Corners Circle – concepts under development, final survey will be complete this week so final designs can be analyzed
7. 15.15 Street Resurfacing – kickoff meeting with Stewart Brothers, 2-17-16

**Work Orders Initiated:**

Order Number	Scheduled	Description	Address	Status Type	Completion
16-001539	2/9/2016	Remove Deceased Animal	6332-6458 Spalding Dr	Completed	2/9/2016
16-001639	3/14/2016	Curb Repair	4271 River Bottom Dr	Completed	3/29/2016
16-001640	3/16/2016	Remove Trash In R.O.W.	Tech Pkwy / Spalding Dr	Completed	3/16/2016
16-001641	3/16/2016	Install Signs	Courtside Dr	Completed	3/22/2016
16-001642	3/17/2016	Install Signs	PIB North & South @ Hwy 141	Completed	3/21/2016
16-001643	3/17/2016	Repair Pothole	Mechanicsville Rd And Peachtree Rd	Pending	
16-001644	3/17/2016	Missing Sign	Old Peachtree Rd At Mechanicsville Rd	Pending	
16-001645	3/17/2016	Repair Pothole	Corner Of Fifth Ave And Florida Ave Nw	Pending	
16-001646	3/18/2016	Street Sign	Bush Rd At Lou Ivy	Completed	4/1/2016
16-001647	3/18/2016	Repair Pothole	3275 Holcomb Bridge Rd	In Progress	
16-001648	3/18/2016	Sidewalk Damage	Jay Bird Alley at Crooked Creek	Completed	3/18/2016
16-001649	3/7/2016	Removed Signs In R.O.W	Jay Bird Alley And Crooked Creek Rd	Completed	3/7/2016
16-001650	3/7/2016	Debris In R.O.W	Eastman Trail And Denton Circle	Completed	3/7/2016
16-001651	3/7/2016	Debris In R.O.W	Denton Circle And Allenhurst Dr	Completed	3/7/2016
16-001652	3/7/2016	Signs	Spalding Dr And Holcomb Bridge Rd	Completed	3/7/2016
16-001653	3/8/2016	Debris In R.O.W	5005 Peachtree Pkwy	Completed	3/8/2016

Order Number	Scheduled	Description	Address	Status Type	Completion
16-001654	3/8/2016	Debris In R.O.W	PIB & S Jones Mill Rd To Greenpointe Pkwy	Completed	3/8/2016
16-001655	3/10/2016	Signs In R.O.W	PIB And S Old Peachtree Rd	Completed	3/10/2016
16-001656	3/11/2016	Pothole Repair	4280 Quail Ridge Way	Completed	3/11/2016
16-001657	3/9/2016	Debris In R.O.W	141 Southbound At Chattahoochee River	Completed	3/9/2016
16-001658	3/14/2016	Debris In R.O.W	Spalding Circle At Peachtree Corners Cir	Completed	3/14/2016
16-001659	3/29/2016	Curb Damage	Tech Parkway	Pending	
16-001660	3/11/2016	Replant Flowers In R.O.W	North & Southbound PIB at Jones Mill	Completed	3/11/2016
16-001661	3/11/2016	Clean Gutters	Southbound PIB At Jones Mill Interchange	Completed	3/11/2016
16-001662	3/23/2016	Remove Debris In R.O.W.	Thamesgate Close	Completed	3/25/2016
16-001663	3/23/2016	Storm Drainage	3551 Cedars Corners Place	Pending	
16-001664	3/23/2016	Storm Drain Issue	3542 Cedar Corners Place	Pending	
16-001665	3/14/2016	Damaged Sign	Ailey Ct At Avera Lane	Completed	3/14/2016
16-001666	3/14/2016	Clean Gutter	141 / Medlock Bridge Rd	Completed	3/14/2016
16-001667	3/14/2016	R.O.W. Landscape Maintenance	141 / Medlock Bridge Rd	Completed	3/14/2016
16-001668	3/14/2016	R.O.W. Landscape Maintenance	141 / Medlock Bridge Rd	Completed	3/14/2016
16-001669	3/15/2016	R.O.W. Landscape Maintenance	Spalding To Riverlake Dr	Completed	3/15/2016
16-001670	3/16/2016	Damaged Sign	141 At Peachtree Corners Circle	Completed	3/16/2016
16-001671	3/16/2016	R.O.W. Landscape Maintenance	Spalding Dr N And Technology Pkwy	Completed	3/16/2016

<b>Order Number</b>	<b>Scheduled</b>	<b>Description</b>	<b>Address</b>	<b>Status Type</b>	<b>Completion</b>
16-001672	3/17/2016	Remove Trash In R.O.W.	141 At Woodhill Dr	Completed	3/17/2016
16-001673	3/17/2016	Installed Signs	141 Southbound At Chattahoochee River and PIB	Completed	3/17/2016
16-001674	3/17/2016	Remove Debris In R.O.W.	PIB Northbound At Winters Chapel	Completed	3/17/2016
16-001675	3/17/2016	R.O.W. Landscape Maintenance	Jones Mill And Governors Lake Dr	Completed	3/17/2016
16-001676	3/18/2016	Remove Trash In R.O.W.	4977 Lou Ivy Rd	Completed	3/18/2016
16-001678	3/18/2016	R.O.W. Landscape Maintenance	Jay Bird Alley/ Spalding Dr To Ptc Cir	Completed	3/18/2016
16-001679	3/18/2016	Remove Trash In R.O.W.	141 And Jay Bird Alley	Completed	3/18/2016
16-001680	3/18/2016	Remove Trash In R.O.W.	141 And Spalding Dr	Completed	3/18/2016
16-001681	3/24/2016	Traffic Signal Out	S.Old Peachtree and Medlock Bridge Rd	Completed	3/25/2016
16-001682	3/18/2016	Remove Deceased Animal	PIB At Medlock Bridge	Completed	3/18/2016
16-001683	3/18/2016	Remove Deceased Animal	PIB S At Jones Mill Rd	Completed	3/18/2016
16-001684	3/18/2016	Remove Trash In R.O.W.	PIB South	Completed	3/18/2016
16-001685	3/18/2016	Remove Trash In R.O.W.	PIB North Access Rd	Completed	3/18/2016
16-001686	3/18/2016	R.O.W. Landscape Maintenance	Peachtree Industrial Blvd North	Completed	3/18/2016
16-001687	3/28/2016	Damaged Street Light	5101 Staverly Lane	Pending	
16-001688	3/18/2016	Pole Installation	4511 Jones Bridge Circle	Completed	3/18/2016
16-001689	3/22/2016	Remove Trash In R.O.W.	PIB Southbound at Greenpoint Pkwy	Completed	3/22/2016
16-001690	3/23/2016	R.O.W. Landscape Maintenance	Medlock Bridge at Bush Rd	Completed	3/23/2016

<b>Order Number</b>	<b>Scheduled</b>	<b>Description</b>	<b>Address</b>	<b>Status Type</b>	<b>Completion</b>
16-001691	3/23/2016	R.O.W Landscape Maintenance	Medlock Bridge At 141	Completed	3/23/2016
16-001692	3/25/2016	Repair Sinkhole	3860 Ancroft Circle	Completed	3/25/2016
16-001693	3/24/2016	R.O.W. Landscape Maintenance	Thamesgate Close	Completed	3/24/2016
16-001694	3/24/2016	R.O.W. Landscape Maintenance	Holcomb Way at Holcomb Bridge Rd	Completed	3/24/2016
16-001695	3/17/2016	R.O.W. Landscape Maintenance	Peachtree Industrial Blvd	Completed	3/17/2016
16-001696	3/17/2016	R.O.W. Landscape Maintenance	Peachtree Industrial Blvd	Completed	3/17/2016
16-001697	3/18/2016	R.O.W. Landscape Maintenance	East Jones Bridge Rd	Completed	3/18/2016
16-001698	3/18/2016	R.O.W. Landscape Maintenance	North And Southbound 141	Completed	3/18/2016
16-001699	3/21/2016	R.O.W. Landscape Maintenance	Medlock Bridge Rd At Bush Rd	Completed	3/21/2016
16-001700	3/21/2016	R.O.W. Landscape Maintenance	141	Completed	3/21/2016
16-001701	3/22/2016	R.O.W Landscape Maintenance	Governors Lake Dr	Completed	3/22/2016
16-001702	3/18/2016	R.O.W Landscape Maintenance	Jones Mill Rd	Completed	3/18/2016
16-001703	3/18/2016	R.O.W. Landscape Maintenance	Park Industrial Dr	Completed	3/18/2016
16-001704	3/23/2016	R.O.W. Landscape Maintenance	Medlock Bridge Rd	Completed	3/23/2016
16-001705	3/23/2016	R.O.W. Landscape Maintenance	141 North & Southbound	Completed	3/23/2016
16-001706	3/24/2016	R.O.W. Landscape Maintenance	Spalding Dr At Peachtree Corners Circle	Completed	3/24/2016
16-001707	3/24/2016	R.O.W. Landscape Maintenance	Jaybird Alley	Completed	3/24/2016
16-001708	3/24/2016	R.O.W. Landscape Maintenance	Holcomb Bridge Rd	Completed	3/24/2016

<b>Order Number</b>	<b>Scheduled</b>	<b>Description</b>	<b>Address</b>	<b>Status Type</b>	<b>Completion</b>
16-001709	3/25/2016	R.O.W. Landscape Maintenance	Hwy 141	Completed	3/25/2016
16-001710	3/25/2016	Removed Deceased Animal	141 And Technology Pkwy	Completed	3/25/2016
16-001711	3/25/2016	R.O.W. Landscape Maintenance	141 Median and Holcomb Bridge Rd	Completed	3/25/2016
16-001712	3/25/2016	R.O.W. Landscape Maintenance	141 Median	Completed	3/25/2016
16-001713	3/21/2016	R.O.W. Landscape Maintenance	Peachtree Ind Blvd North	Completed	3/21/2016
16-001714	3/21/2016	R.O.W. Landscape Maintenance	Peachtree Ind Blvd	Completed	3/21/2016
16-001715	3/22/2016	R.O.W. Landscape Maintenance	Jones Mill Rd	Completed	3/22/2016
16-001716	3/22/2016	R.O.W. Landscape Maintenance	Governors Lake Dr	Completed	3/23/2016
16-001717	3/23/2016	High Grass/Weeds	Medlock Bridge Rd At 141	Completed	3/23/2016
16-001718	3/23/2016	Remove Debris In R.O.W	Technology Pkwy	Completed	3/23/2016
16-001719	3/23/2016	R.O.W. Landscape Maintenance	Holcomb Bridge Rd	Completed	3/23/2016
16-001720	3/24/2016	R.O.W. Landscape Maintenance	Peachtree Ind Blvd	Completed	3/24/2016
16-001721	3/23/2016	Remove Debris In R.O.W	Governors Lake Dr	Completed	3/23/2016
16-001722	3/21/2016	Repair Curb	5001 Gallatree Lane	Completed	3/29/2016
16-001723	4/7/2016	Damaged Sign	3200 Pointe Pkwy	Pending	
16-001724	4/8/2016	Removed Deceased Animal	5051 Peachtree Ind Blvd	Completed	4/8/2016
16-001725	3/28/2016	R.O.W. Landscape Maintenance	Spalding Drive And Crooked Creek	Completed	3/28/2016
16-001726	3/28/2016	R.O.W. Landscape Maintenance	Winters Chapel Rd	Completed	3/28/2016

<b>Order Number</b>	<b>Scheduled</b>	<b>Description</b>	<b>Address</b>	<b>Status Type</b>	<b>Completion</b>
16-001727	3/29/2016	Removed Trash In R.O.W	Holcomb Bridge Rd	Completed	3/29/2016
16-001728	3/29/2016	High Grass/Weeds	Medlock Bridge Rd	Completed	3/29/2016
16-001729	3/30/2016	R.O.W Landscape Maintenance	Peachtree Ind Blvd Southbound	Completed	3/30/2016
16-001730	3/30/2016	R.O.W. Landscape Maintenance	Peachtree Ind Blvd	Completed	3/30/2016
16-001731	3/31/2016	R.O.W. Landscape Maintenance	Hwy 141	Completed	3/31/2016
16-001732	4/1/2016	Removed Trash In R.O.W	Peachtree Ind Blvd	Completed	4/1/2016
16-001733	4/1/2016	Removed Deceased Animal	Peachtree Ind Blvd	Completed	4/1/2016
16-001734	4/1/2016	Removed Deceased Animal	Holcomb Bridge Rd	Completed	4/1/2016
16-001735	4/8/2016	Removed Object In R.O.W.	5020 Bankside Way	In Progress	
16-001736	4/8/2016	Remove Object In R.O.W	4891 Bankside Way	In Progress	
16-001737	4/7/2016	Street Light Repair	Triangle Pwky/ Triangle Dr	Pending	
16-001738	4/1/2016	Remove Trash In R.O.W.	E. Jones Bridge Rd	Completed	4/1/2016
16-001739	4/1/2016	Removed Deceased Animal	Peachtree Ind Blvd	Completed	4/1/2016
16-001740	4/1/2016	Remove Trash In R.O.W.	West Jones Bridge Rd	Completed	4/1/2016
16-001741	4/1/2016	Removed Trash In R.O.W.	Fitzgerald Rd	Completed	4/1/2016
16-001742	4/4/2016	R.O.W. Landscape Maintenance	Peachtree Ind Blvd	Completed	4/4/2016
16-001743	4/4/2016	Landscape Maintenance	Park Industrial Dr	Completed	4/4/2016
16-001744	4/5/2016	High Grass/Weeds	Jay Bird Alley	Completed	4/5/2016
16-001745	4/5/2016	High Grass / Weeds	Hwy 141	Completed	4/5/2016

Order Number	Scheduled	Description	Address	Status Type	Completion
16-001746	4/7/2016	High Grass/Weeds	Peachtree Ind Blvd	Completed	4/7/2016
16-001747	4/4/2016	Removed Trash In R.O.W	Peachtree Corners Circle	Completed	4/4/2016
16-001748	4/4/2016	Removed Trash In R.O.W	PIB And Winters Chapel	Completed	4/4/2016
16-001749	4/4/2016	Removed Trash In R.O.W	Hwy 141/ Chattahoochee River	Completed	4/4/2016
16-001750	4/4/2016	Eliminate Weeds In R.O.W.	Hwy 141/ E Jones Bridge Rd	Completed	4/4/2016
16-001751	4/4/2016	Eliminate Weeds In R.O.W.	Hwy 141/ Chattahoochee River	Completed	4/4/2016
16-001752	4/5/2016	Damaged Sign	Lou Ivy And South Old Peachtree Rd	Completed	4/5/2016
16-001753	4/5/2016	High Grass/Weeds	Peachtree Ind Blvd	Completed	4/5/2016
16-001754	4/5/2016	Landscape Maintenance	Park Industrial Dr	Completed	4/5/2016
16-001755	4/5/2016	Eliminated Weeds In R.O.W	W. Jones Bridge Rd/ Peachtree Cnrs Cir	Completed	4/5/2016
16-001756	3/14/2016	Repair Curb	4271 River Bottom Dr	Completed	3/31/2016
16-001757	3/27/2016	Repair Potholes	6640 Lockridge Dr	Completed	3/29/2016
16-001758	3/31/2016	Remove Trash In R.O.W.	141 at Peachtree Corners Circle	Completed	3/31/2016
16-001759	4/5/2016	Remove Debris In R.O.W.	PIB at Winters Chapel Rd	Completed	4/5/2016
16-001760	3/31/2016	Remove Trash In R.O.W.	East Jones Bridge Rd	Completed	3/31/2016
16-001761	4/6/2016	Clean Median Walls	PIB NB/ Winters Chapel/Jimmy Carter Blvd	Completed	4/6/2016
16-001762	4/7/2016	R.O.W. Landscape Maintenance	PIB At Winters Chapel	Completed	4/7/2016
16-001763	4/8/2016	Removed Trash In R.O.W	Tech Parkway	Completed	4/8/2016

<b>Order Number</b>	<b>Scheduled</b>	<b>Description</b>	<b>Address</b>	<b>Status Type</b>	<b>Completion</b>
16-001764	4/8/2016	Remove Trash In R.O.W.	5277 Peachtree Pkwy	Completed	4/8/2016
16-001765	4/8/2016	Cleared Storm Drain	PIB Northbound At Gateway Dr	Completed	4/8/2016
16-001766	4/8/2016	Damaged Sign	S Old Peachtree Rd At PIB	Completed	4/8/2016
16-001767	4/8/2016	Remove Deceased Animal	Spalding Dr at Neely Farm Rd	Completed	4/8/2016
16-001768	4/7/2016	Remove Trash In R.O.W.	Technology Pkwy / Spalding Dr	Completed	4/7/2016
16-001769	4/7/2016	Damaged Sign	3200 Pointe Pkwy	Completed	4/7/2016
16-001770	4/8/2016	Removed Trash In The R.O.W.	Technology Pkwy/ Scientific Dr	Completed	4/8/2016
16-001771	4/8/2016	Removed Trash In R.O.W.	141 Northbound	Completed	4/8/2016
16-001772	4/8/2016	R.O.W. Landscape Maintenance	Holcomb Bridge Rd	Completed	4/8/2016
16-001773	4/8/2016	Landscape Maintenance In The R.O.W.	PIB North Winters Chapel	Completed	4/8/2016
16-001774	4/8/2016	R.O.W. Landscape Maintenance	Jones Mill / PIB	Completed	4/8/2016
16-001775	4/8/2016	Remove Trash In R.O.W.	PIB Northbound/ Jones Mill Rd	Completed	4/8/2016

**Work Orders Referred To Other Departments:**

<b>Date Created</b>	<b>Request Type</b>	<b>Address</b>	<b>Status Type</b>	<b>Referred To Other Departments</b>
3/10/2016	Pothole #219833	4048 Peachtree Corners Circle	In-process	Gwinnett County DOT
3/21/2016	Pothole ID# 894761	3275 Holcomb Bridge Road	In-process	Gwinnett County DOT Service Request # 16-001647
3/23/2016	Storm Drain Issue	3551 Cedar Corners Place	In-process	Gwinnett County DWRS Service Request 16-002613
3/23/2016	Storm Drain Issue	3542 Cedar Corners Place	In-process	Gwinnett County DWRS Service Request 16-002613
3/23/2016	Storm Drain Issue	3691/3681/3671 Cedar Corners Place	In-process	Gwinnett County DWRS Service Request 16-002627
3/25/2016	Traffic Signal Out ID# 222950	Medlock Bridge Rd/ S. Old Peachtree Rd	In-process	Gwinnett County DOT
3/28/2015	Street Light Out #223296	5101 Staverly Lane	Complete	GA Power # 2061156
4/4/2016	Traffic Light Sensors	Vicinity of West Jones Bridge Rd	In-process	Gwinnett County DOT Service Request # 896884
4/8/2016	Street Light Out #225814	3739 Triangle Dr NW	In-process	GA Power # 2075519

**02016-04-71**

**AN ORDINANCE TO AMEND THE CITY OF PEACHTREE CORNERS ZONING MAP PURSUANT TO RZ2016-003, SOUTH BERKELEY LAKE RD. AUTO STORAGE REQUEST TO REZONE FOR AN AUTO STORAGE FACILITY ON 4.09 ACRES LOCATED AT 4684 SOUTH BERKELEY LAKE ROAD IN DISTRICT 6, LAND LOTS 269, PEACHTREE CORNERS, GA**

**WHEREAS:** Notice to the public regarding said modification to conditions of zoning has been duly published in The Gwinnett Daily Post, the Official News Organ of Peachtree Corners; and

**WHEREAS:** Public Hearings were held by the Mayor and City Council of Peachtree Corners on April 19, 2016 and May 17, 2016;

**NOW THEREFORE,** The Mayor and City Council of the City of Peachtree Corners while in Regular Session on May 17, 2016 hereby ordain and approve the Zoning Case RZ2016-003 for the above referenced property with the following enumerated conditions:

- 1) The property shall be rezoned M-2; however, only truck storage shall be permitted.
- 2) Truck storage shall not be located within the front yard, (the 50 feet between the front property line and the front building setback line).
- 3) The three large trees located between the existing house and the front property line shall be preserved. If encroachment into the critical root zone of these trees is necessary, a maximum of 20% encroachment shall be permitted and such encroachment may only be with permeable or pervious pavers.
- 4) The vegetation within the north side setback area shall remain undisturbed except for the minimum clearing necessary to install the stormwater system and to accommodate the 10 parking spaces closest to the northern property line.
- 5) The fence along the northeast corner of the property shall be relocated from where it's shown on the plans prepared by Day Design Group dated 2/26/15 and placed immediately behind the dumpster instead of running all the way to the corner of the property. The placement of the fence in this manner is for the purpose of preserving all the existing vegetation on the northeast corner of the property.
- 6) The existing vegetation within the 15 ft. rear setback shall remain undisturbed.
- 7) The property shall be screened along the front and the first 100 ft. along the north and south sides by an 8' tall solid wood fence, with brick and stone accent posts placed a minimum of 25 feet on center. The exact placement of the fence along the front of the property shall be determined by Staff after a survey showing the location of the trees and CRZs is submitted. Every effort shall be made to minimize the impact on the trees when locating and installing the fence.
- 8) Landscape strip along the front of the property shall meet overlay standards except that existing mature vegetation at the front may also be utilized to meet the requirements.

- 9) The remainder of the property may use an 8' tall slatted chain-link fence.
- 10) No inoperable (junk/salvage) vehicles shall be stored on site.
- 11) No vehicles shall be located on unpaved surfaces.
- 12) The materials used for the guard house shall match the materials used for the front fence.
- 13) A 5 ft. wide sidewalk shall be installed along South Berkley Lake Road.
- 14) No billboards shall be permitted on the property.
- 15) Outdoor lighting shall be contained in cutoff-type luminaries and shall be directed inward toward the property so as not to reflect into adjacent properties or to create a hazard for passing automobile traffic.
- 16) Permanent freestanding project signage shall be limited to one monument sign to be located at the front of the property.
- 17) All conditions must be met before a business license can be issued.

Effective this 17th day of May, 2016.

So signed and Witnessed

Approved :

this \_\_\_\_\_ day of \_\_\_\_\_, 2016  
Attest:

\_\_\_\_\_  
Kymerly Chereck, City Clerk

\_\_\_\_\_  
Mike Mason, Mayor

**R2016-04-59**

**J. Putnam**

**A RESOLUTION TO ADOPT GUIDELINES FOR POSTING MATERIAL ON  
SOCIAL MEDIA PLATFORMS AND ACCOUNTS MAINTAINED BY THE  
CITY OF PEACHTREE CORNERS**

**WHEREAS**, the City is a municipal corporation, and uses social media to keep its citizens informed of news and events involving the City and its residents; and

**WHEREAS**, the Mayor and Council desire to adopt policies governing the posting of material across the several social media platforms used by the City, now and hereafter, including without limitation Facebook, Twitter, and the City's Community Calendar.

**NOW THEREFORE**, be it **RESOLVED** by the Mayor and City Council for the City of Peachtree Corners, Georgia, that the following Posting Guidelines are hereby adopted to govern postings across the social media platforms used by the City:

**Posting Guidelines for the City of Peachtree Corners  
Community Calendar and Social Media**

The city's social media page(s) announce events of general public interest in Peachtree Corners that are sponsored by non-profit organizations.

- Only submissions by nonprofit organizations will be considered.
- Submissions must be for events open to the public and not just to members of the submitting organization.
- As our jurisdiction is limited to the City of Peachtree Corners, supported events must take place in Peachtree Corners. However, an exception will be made for the following schools that are located outside the city limits, but are attended by many who live in the city:
  - Berkeley Lake Elementary School
  - Stripling Elementary
  - Duluth Middle School
  - Summerour Middle
  - Duluth High
  - Norcross High
- If an event is approved for posting, it does not constitute endorsement by the City nor give permission to use City resources for the event. Please be advised that a separate facilities or event permit may be required.
- Gwinnett County activities, notices, etc. that affect Peachtree Corners are permitted.

As a .gov website, we cannot advertise for private individuals, firms, corporations, or imply in any manner that we endorse or favor any specific commercial product, commodity, service or business. We also cannot promote events considered to be religious or political in nature, or of interest to a specific religion or political party.

We do not list estate or garage sales run by individuals.

We reserve the right to refuse any submission.

Links to external sites

At its discretion, the city may provide links to external sites that fall into one of the following categories:

- Businesses with active Peachtree Corners business tax certificates.
- City of Peachtree Corners created pages on other social media websites, such as YouTube and Twitter.
- News organizations serving Peachtree Corners.
- Non-profit organizations that support the city's strategic or operational goals.
- Organizations that partner with the City of Peachtree Corners to meet the city's strategic or operational goals.
- Other governmental agencies.
- Public and private, bona fide educational institutions.
- Utilities that provide service in the city.

**RESOLVED** this the 19<sup>th</sup> day of April, 2016.

**Approved:**

\_\_\_\_\_  
Mike Mason, Mayor

ATTEST:

\_\_\_\_\_(SEAL)  
Kym Chereck, City Clerk

**R2106-04-60**

**D. Wheeler &**

**P. Floyd**

**RESOLUTION OF CITY OF PEACHTREE CORNERS, GEORGIA APPROVING  
AMENDMENT OF AN INTERGOVERNMENTAL CONTRACT RESPECTING  
THE TOWN CENTER PROJECT WITH THE DOWNTOWN DEVELOPMENT  
AUTHORITY OF THE CITY OF PEACHTREE CORNERS EXTENDING THE  
TERM THEREOF, AND FOR OTHER PURPOSES**

WHEREAS, the Downtown Development Authority of the City of Peachtree Corners (the “Authority”) was duly created and is validly existing as a public body corporate and politic created pursuant to the Downtown Development Authorities Law (O.C.G.A. Section 36-42-1 *et seq.*); and

WHEREAS, the Authority was created for the purposes of developing and promoting for the public good and general welfare, trade, commerce, industry and employment opportunities and to promote the general welfare of the City of Peachtree Corners, Georgia (the “City”) and the State of Georgia; and

WHEREAS, the Act grants the Authority the power and authority (a) to acquire by purchase, lease, or otherwise and to hold, lease, and dispose of real and personal property of every kind an character, or any interest therein, in furtherance of the public purpose of the Authority; (b) to borrow money to further carry out its public purpose and to execute and sell notes as may be necessary or desirable, in the judgment of the Authority, to evidence and to provide security for the borrowing; (c) to issue notes of the Authority and use the proceeds thereof for the purpose of paying, or loaning the proceeds thereof to pay, all or any part of the cost of any project and otherwise to further or carry out the public purpose of the Authority and to pay all costs of the Authority incidental to, or necessary and appropriate to, furthering or carrying out such purpose; and (d) to receive and use the proceeds of any tax levied by a municipal corporation to pay the costs of any project or for any other purpose for which the Authority may use its own funds; and

WHEREAS, Article IX, Section III, Paragraph I(a) of the Constitution of the State of Georgia authorizes, among other things, any county, municipality or other political subdivision of the State to contract, for a period not exceeding fifty years, with another county, municipality or political subdivision or with any other public agency, public corporation or public authority for joint services, for the provision of services, or for the provision or separate use of facilities or equipment, provided that such contract deals with activities, services or facilities which the contracting parties are authorized by law to undertake or to provide; and

WHEREAS, the City is a municipal corporation of the State of Georgia, legally created and validly existing under the laws of the State of Georgia; and

WHEREAS, in accordance with Official Code of Georgia Annotated, Section 48-5-350, the City may levy and collect municipal taxes upon all taxable property within the limits of the municipality to provide for financial assistance to its development authority for the purpose of developing trade, commerce, industry and employment opportunities; provided, however, that the tax levied for such purposes shall not exceed three (3) mills per dollar upon the assessed value of such property; and

WHEREAS, the Charter of the City provides that the City shall have the power to levy and provide for the assessment, valuation, revaluation, and collection of taxes on all property subject to taxation subject to a maximum of one (1) mill; and

WHEREAS, the City owns certain real property consisting of 19.689 acres on Peachtree Parkway and Medlock Bridge Road within the City for the essential public purpose of the development of trade, commerce, industry, and employment opportunities (the "Property"); and

WHEREAS, the Authority obtained a loan (the "Loan") from Ameris Bank (the "Bank") in the amount of \$11,500,000 secured by the Property in order to (a) refinance of the Property, and (b) pay the costs incurred in connection with the Loan; and

WHEREAS, the Authority executed a taxable promissory note in the principal amount of \$11,500,000 (the "Note") for the purpose of evidencing the Loan; and

WHEREAS, the Authority and the City entered into an intergovernmental contract, dated May 13, 2015, between the Authority and the City (the "Contract"), in connection with the Loan, pursuant to which the Authority agreed to enter into the Loan and operate the Property, and the City, in consideration of such services provided by the Authority, agreed to pay to the Authority amounts sufficient to pay the principal of and interest on the Note as the same become due, to the extent the revenues from the Property are insufficient (the "Contract Payments"); and

WHEREAS, the Authority assigned its rights in the Contract and the Contract Payments to the Bank, and the City consented to such assignment and agreed to certain covenants with the Bank, pursuant to an Assignment and Agreement Regarding Intergovernmental Contract, dated May 13, 2015 (the "Assignment"), executed by the Authority, the City and the Bank, and

WHEREAS, the Authority approved (a) the Loan and (b) the execution and delivery of the Contract and the Assignment pursuant to its resolution adopted on April 21, 2015, as amended and supplemented, including by this resolution (the "Resolution"); and

WHEREAS, the Authority, the City, and the Bank desire to extend the maturity date of the Note by one year, from May 13, 2016 to May 13, 2017, through a Loan Modification Agreement and such extension requires the execution of an amendment to the Contract and other documentation as deemed necessary; and

WHEREAS, the Mayor and Council of the City of Peachtree Corners, Georgia (the “Mayor and City Council”) has determined it to be in the best interest of the citizens of the City for the Authority to finance or refinance the Property by the extension of the term of the Note, secured by the Contract, so that the Authority can continue to develop and promote trade, commerce, industry and employment opportunities for the public good and general welfare of the citizens of the City and to promote the general welfare of the State of Georgia;

NOW, THEREFORE, BE IT RESOLVED BY THE MAYOR AND COUNCIL OF THE CITY OF PEACHTREE CORNERS, GEORGIA, as follows:

Section 1. The City hereby approves and authorizes the execution, delivery and performance of an amendment to the Contract, on behalf of the City, with the Authority substantially in the form attached hereto as Exhibit A (the “Contract”), including the exhibits thereto, subject to such changes, additions and deletions made in the discretion of the Mayor (the “Authorized Official”) of the City, with advice of counsel. The Contract shall be executed by the Authorized Official, attested by the appropriate officer of the City, and shall have the City's seal affixed, and when so executed and delivered, shall be binding upon the City in accordance with its terms. Execution of the Contract as authorized herein shall be conclusive evidence of the City’s approval thereof.

Section 2. The City hereby authorizes the Authorized Official and City Manager, or either of them, to take any further actions and execute and deliver any other documents necessary to carry out the purpose of the Resolution, this resolution and the draft Loan Modification Agreement between the Authority and the Bank, which is attached hereto as Exhibit B.

Section 3. All resolutions or parts of resolutions in conflict herewith are hereby repealed.

SO RESOLVED, this 19th day of April, 2016.

CITY OF PEACHTREE CORNERS,  
GEORGIA

By: \_\_\_\_\_  
Mike Mason, Mayor

(SEAL)

Attest: \_\_\_\_\_  
Kym Chereck, City Clerk

EXHIBIT "A"

First Amendment to Intergovernmental Contract

Draft 4/14/16

**FIRST AMENDMENT TO INTERGOVERNMENTAL CONTRACT**

**Between**

**DOWNTOWN DEVELOPMENT AUTHORITY  
OF THE CITY OF PEACHTREE CORNERS**

**and**

**CITY OF PEACHTREE CORNERS**

This **FIRST AMENDMENT TO CONTRACT**, made and entered into as of April 19, 2016, by and between **DOWNTOWN DEVELOPMENT AUTHORITY OF THE CITY OF PEACHTREE CORNERS**, a public body corporate and politic (the "Authority"), and the **CITY OF PEACHTREE CORNERS**, a municipal corporation of the State of Georgia (the "City").

**WITNESSETH:**

**WHEREAS**, the Authority was duly created and is validly existing pursuant to the Downtown Development Authorities Law of the State of Georgia (O.C.G.A. Section 36-42-1 et seq.), as amended (the "Act"); and

**WHEREAS**, under the Act, the Authority has, among others, the power to (a) acquire and operate "projects" (as defined in the Act), (b) borrow money for corporate purposes and (c) issue notes for the purpose of providing funds to carry out the duties of the Authority; and

**WHEREAS**, Article IX, Section III, Paragraph I(a) of the Constitution authorizes, among other things, any county, municipality or other political subdivision of the State to contract, for a period not exceeding fifty years, with another county, municipality or political subdivision or with any other public agency, public corporation or public authority for joint services, for the provision of services, or for the provision or separate use of facilities or equipment, provided that such contract deals with activities, services or facilities which the contracting parties are authorized by law to undertake or to provide; and

**WHEREAS**, the Authority executed a taxable promissory note (the “Note”) from AMERIS BANK (the “Bank”) (the legal holder from time to time of the below-described Note, including the Bank as the initial holder, is hereinafter referred to as the “Holder”) in the amount of \$11,500,000.00 in order to (a) finance the acquisition of certain real property for the essential public purpose of the development of trade, commerce, industry, and employment opportunities (the “Property”), and (b) pay the costs incurred in connection with the Loan; and

**WHEREAS**, the Authority has approved the Note pursuant to its resolution adopted on May 13, 2015, as supplemented and amended (the “Resolution”); and

**WHEREAS**, the Authority and the City entered into an Intergovernmental Contract on May 13, 2015, in connection with the Note, pursuant to which the Authority agreed to issue the Note and operate the Property, and the City, in consideration of such services provided by the Authority, agreed to pay to the Authority amounts sufficient to pay (i) the principal of, prepayment premium (if any) and interest on the Note as the same become due, and (ii) all other amounts owing under the Note (all amounts in (i) and (ii) are hereafter collectively the “Contract Payments”); and

**WHEREAS**, as a condition to obtaining the Note, the Authority transferred and assigned all of the Authority’s rights in and to the Contract and the Contract Payments to the Bank pursuant to an assignment instrument, dated the date of its execution and delivery (the “Assignment”), executed by the Authority in favor of the Bank.

**WHEREAS**, the Authority and Bank intend to enter into a Loan Modification Agreement modifying the Note to extend the initial maturity date set forth therein from May 13, 2016 to May 13, 2017;

**NOW, THEREFORE**, for and in consideration of the premises and undertakings as hereinafter set forth and other good and valuable consideration, the receipt and sufficiency of which are hereby acknowledged, the City and the Authority **DO HEREBY AGREE**, as follows:

**Section 1.** Section 1 of the Intergovernmental Contract is hereby amended to modify the definition of (a) “Note” to refer to the Note as amended, modified and supplemented to extend the maturity date of the Note by one year, from May 13, 2016 to May 13, 2017, (b) “Loan” to refer to the Loan as amended, modified and supplemented by the Loan Modification Agreement, dated as of May \_\_, 2016, between the Authority and Bank, for the purpose of extending the maturity date of the Note by one year, from May 13, 2016 to May 13, 2017, and (c) “Resolution” to refer to the Resolution as amended, modified and supplemented by the resolution of the City adopted on April 19, 2016 authorizing this Amendment.

**Section 2.** This First Amendment shall be read and taken together with the Intergovernmental Contract as one and the same instrument. The Intergovernmental Contract, as amended by this First Amendment, is hereby ratified and affirmed in all respects.

IN WITNESS WHEREOF, the parties hereto, acting by and through their duly authorized officers, have caused this Contract to be executed in multiple counterparts under seals as of the day and year first above written.

(SEAL)

CITY OF PEACHTREE CORNERS,  
GEORGIA

Attest:

By: \_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk

APPROVED:

By: \_\_\_\_\_  
City Attorney

(SEAL)

DOWNTOWN DEVELOPMENT  
AUTHORITY OF THE CITY OF  
PEACHTREE CORNERS, GEORGIA

Attest:

By: \_\_\_\_\_  
Chairman

\_\_\_\_\_  
Secretary

EXHIBIT "B"

Loan Modification Agreement

Draft 4/14/16

(The space hereinabove is for recordation purposes only)

---

Prepared by and Return to:  
North Atlanta Law Group, PC  
2475 Northwinds Parkway, Suite 130  
Alpharetta, Georgia 30009  
File Number 4752.5

MODIFICATION OF LOAN DOCUMENTS

STATE OF GEORGIA

PLEASE CROSS INDEX TO:

a. Page 785,

COUNTY OF FULTON

Deed Book 53556,

Gwinnett County, Georgia records

b.

**THIS MODIFICATION OF LOAN DOCUMENTS** (the "Modification") entered into this \_\_\_ day of May, 2016, by and among **Downtown Development Authority of the City of Peachtree Corners, a public body corporate and politic created and existing under the Downtown Development Authorities Law** (hereinafter referred to as "Borrower"), and **AMERIS BANK** (hereinafter referred to as "Lender").

WITNESSETH:

**WHEREAS**, Borrower executed and delivered to Lender a Deed to Secure Debt, and Security Agreement, dated May 13, 2015, and recorded May 14, 2015, in Deed Book 53556, Page 785, of Gwinnett County, Georgia Records, (hereinafter referred to as the "Mortgage") to secure a promissory note of even date in the original principal sum of \$11,500,000.00 (hereinafter referred to as the "Note"), secured by real estate more particularly described on Exhibit "A" attached hereto and made a part hereof by reference (hereinafter referred to as the "Property");

**WHEREAS**, Borrower, on May 13, 2015, further executed the following documents: Assignment of Revenues, Loan Agreement and other documents incidental to the May 13, 2015 closing, (hereinafter collectively with the Note and Mortgage shall be referred to as the "Loan Documents");

**WHEREAS**, Borrower and Guarantor have requested that Lender modify the Loan Documents to change the initial maturity date set forth therein to May 13, 2017;

**WHEREAS**, Borrower is entering into this Modification to evidence its acknowledgement, agreement and acquiescence to each and every term, provision, and condition contained herein; and

**WHEREAS** the Loan Documents, and this Modification shall collectively be referred to as the "Amended Loan Documents"; and

**WHEREAS**, the Borrower acknowledges and agrees that they shall be liable under the Amended Loan Documents as may be further amended and that the obligations thereunder are in no way released, diminished, or impaired hereby.

**NOW THEREFORE**, in consideration of the mutual covenants and agreements, and the mutual benefits to be gained by the performance hereof, and in consideration of Ten Dollars (\$10.00) paid by Borrower to Lender, and for other good and valuable considerations, the receipt and sufficiency of which are hereby acknowledged by each party, the parties hereto, for themselves, their successors and assigns, hereby covenant and agree as follow:

1.

Borrower hereby represents and acknowledges that all of the statements set forth in the recital paragraphs are true and accurate, and such paragraphs are hereby incorporated into this Agreement by reference.

2.

The Borrower and Lender each acknowledge and agree that the Maturity Date set forth in the Loan Documents shall be amended to May 13, 2017.

3.

The Borrower and Lender each acknowledge and agree that the terms, provisions and conditions of and within this Modification supersede and take precedence over any conflicting provisions contained in the Amended Loan Documents. The Amended Loan Documents shall have the same force and effect as if the amendments as herein provided had been set forth therein when said Loan Documents were originally executed, delivered and recorded. All remaining terms, provisions and conditions of the Amended Loan Documents (that are not specifically modified herein) remain in full force and effect.

4.

The Borrower hereby (a) ratifies and confirms the terms and conditions of the Note and the Amended Loan Documents; (b) acknowledges that the Note and the Amended Loan Documents are in full force and effect; (c) consents to the provisions of this Modification; and (d) covenants that the Borrower has no defenses or offsets with respect to the obligations under the Amended Loan Documents and no counterclaims or causes of action of any kind of nature whatsoever by Borrower against the Lender or any of Lender's directors, officers, employees, agents or attorneys whether based upon the transactions evidenced by the Amended Loan Documents or otherwise, including but not limited to any defenses under O.C.G.A. § 10-7-21 or 10-7-22.

5.

This Modification shall be binding upon and inure to the benefit of the parties hereto and their respective heirs, executors, legal representatives, successors and assigns.

6.

The Borrower hereby releases and forever discharges Lender, its directors, officers, employees, agents, assigns, attorneys, accountants, successors, predecessors, representatives and others who may have acted or been claimed to have acted in concert with Lender, from any and all charges, complaints, claims, liabilities, obligations, promises, agreements, controversies, demands, damages, actions, causes of actions or suits of any kind or nature whatsoever, whether they are found in equity, law, tort or contract, arising out of any act or omission of Lender or such other persons, whether known or unknown, occurring at any time before the date of this Modification.

7.

Nothing hereby contained shall be construed as a novation or a default in any of the terms and conditions of the Note or the Amended Loan Documents executed and/or modified by the undersigned parties. Notwithstanding the foregoing, in the event that any of

the prior events or anything referenced in this Modification shall have caused or be deemed a novation, Note and the Mortgage shall immediately become due and payable.

IN WITNESS WHEREOF, the parties hereto have executed and sealed this Modification the day, month and year first above written.

Signed, sealed and delivered in the presence of:

**DOWNTOWN DEVELOPMENT  
AUTHORITY OF THE CITY OF  
PEACHTREE CORNERS**, a public body corporate and politic created under the laws of the State of Georgia

\_\_\_\_\_  
Unofficial Witness

By: \_\_\_\_\_  
Dan Graveline

\_\_\_\_\_  
Notary Public

[NOTARIAL SEAL]

Attest: \_\_\_\_\_  
Diana Wheeler  
Secretary

Commission Expire Date:  
\_\_\_\_\_

[AUTHORITY SEAL]

THE WITHIN AND FOREGOING MODIFICATION OF LOAN DOCUMENTS IS ACCEPTED THIS 13<sup>TH</sup> DAY OF MAY, 2016.

**LENDER:**

**AMERIS BANK**

By: \_\_\_\_\_  
Michael Tesler  
Market President

[BANK SEAL]

Signed, sealed and delivered in the presence of:

\_\_\_\_\_  
Witness

\_\_\_\_\_  
Notary Public

**EXHIBIT "A"**  
**Legal Description**

All that tract or parcel of land lying and being in the City of Peachtree Corners, Land Lot 301 of the 6th Land District, Gwinnett County, Georgia and being more particularly shown as Tract Two (20.609 acres) on that certain Final Plat for Roberts Properties Residential, L.P., prepared by Randall W. Dixon, Georgia Registered Land Surveyor No. 1678 of Precision Planning, Inc., dated September 12, 2012, filed September 25, 2012, and recorded in Plat Book 129, pages 136 and 137, Gwinnett County, Georgia records, and incorporated herein by this reference.

TOGETHER WITH all rights, title and interest in and to that certain easement contained in that certain Grant of Easements from J. Alexander's Restaurants, Inc., a Tennessee corporation, to Roberts Properties Residential, L.P., a Georgia limited partnership, dated January 28, 2013, filed February 8, 2013, and recorded in Deed Book 52000, page 631, Gwinnett County, Georgia records (the "Sewer Easement").

FURTHER TOGETHER WITH easements under that certain Access Easement and Landscaping Agreement between Roberts Properties Residential, L.P. and Piedmont Five, LLC, dated September 27, 2012, filed October 1, 2012, recorded in Deed Book 51679, page 671, Gwinnett County, Georgia records; as amended by First Amendment to Access Easement and Landscaping Agreement dated November 5, 2013, recorded December 6, 2013, in Deed Book 52682, page 89, aforesaid records; as further amended by Second Amendment to Access Easement and Landscaping Agreement among Downtown Development Authority of the City of Peachtree Corners, CF Peachtree Corners Investments, LLC, VAL Peachtree Corners, LLC, dated February 20, 2015, recorded February 23, 2015, in Deed Book 53393, page 906, aforesaid records.

LESS AND EXCEPT THE FOLLOWING PROPERTY:

All that tract or parcel of land lying and being in Land Lot 301 of the 6th District, Gwinnett County, Georgia, and being more particularly described as follows:

COMMENCING at a 1/2-inch rebar found at the intersection of the Southeasterly right-of-way line of Peachtree Parkway, a.k.a. Georgia Hwy 141 (variable right-of-way) and the Southwesterly right-of-way line of Medlock Bridge Road, (variable right-of-way); Thence along said Southwesterly right-of-way line of Medlock Bridge Road, South 28 degrees 55 minutes 58 seconds East, a distance of 352.48 feet to a 1.5-inch open top pipe found; Thence South 30 degrees 19 minutes 01 seconds East, a distance of 135.00 feet to a 1/2-inch rebar found, said point being the TRUE POINT OF BEGINNING; Thence continue along said right-of-way, South 30 degrees 19 minutes 01 seconds East, a distance of 165.05 feet to a point; Thence departing said right-of-way, South 6 degrees 54 minutes 16 seconds West, a distance of

207.92 feet to a point; Thence North 36 degrees 08 minutes 48 seconds West, a distance of 196.72 feet to a point; Thence North 34 degrees 57 minutes 42 seconds East, a distance of 15.22 feet to a point; Thence North 66 degrees 51 minutes 51 seconds East, a distance of 31.00 feet to a point; Thence North 66 degrees 51 minutes 51 seconds East, a distance of 184.52 feet to a 1/2-inch rebar found, said point being the TRUE POINT OF BEGINNING.

Said tract of land contains 0.920 Acres.

**R2016-04-61**

**B. Branham**

GEORGIA CITIES WEEK  
April 17-23, 2016

A RESOLUTION OF THE CITY OF PEACHTREE CORNERS  
RECOGNIZING GEORGIA CITIES WEEK, APRIL 17-23, 2016 AND  
ENCOURAGING ALL CITIZENS TO SUPPORT THE CELEBRATION AND  
CORRESPONDING ACTIVITIES.

WHEREAS, city government is the closest to most citizens, and the one with the most direct daily impact upon its residents; and

WHEREAS, city government is administered for and by its citizens, and is dependent upon public commitment to and understanding of its many responsibilities; and

WHEREAS, city government officials and employees share the responsibility to pass along their understanding of public services and their benefits; and

WHEREAS, Georgia Cities Week is a very important time to recognize the important role played by city government in our lives; and

WHEREAS, Georgia Cities Week offers an important opportunity to spread the word to all the citizens of Georgia that they can shape and influence this branch of government which is closest to the people; and

WHEREAS, the Georgia Municipal Association and its member cities have joined together to teach students and other citizens about municipal government through a variety of different projects and information; and

WHEREAS, Georgia Cities Week offers an important opportunity to convey to all the citizens of Georgia that they can shape and influence government through their civic involvement.

NOW, THEREFORE BE IT RESOLVED THAT THE CITY OF PEACHTREE CORNERS DECLARES APRIL 17-23, 2016 AS GEORGIA CITIES WEEK.

BE IT FURTHER RESOLVED THAT THE CITY OF PEACHTREE CORNERS ENCOURAGES ALL CITIZENS, CITY GOVERNMENT OFFICIALS AND EMPLOYEES TO DO EVERYTHING POSSIBLE TO ENSURE THAT THIS WEEK IS RECOGNIZED AND CELEBRATED ACCORDINGLY.

CITY OF PEACHTREE CORNERS  
COUNTY OF GWINNETT, STATE OF GEORGIA

RESOLUTION 2016-04-61

**RESOLVED** this the 19<sup>th</sup> day of April, 2016.

**Approved:**

\_\_\_\_\_  
Mike Mason, Mayor

ATTEST:

\_\_\_\_\_(SEAL)  
Kym Chereck, City Clerk

**Action Item**  
**Street Resurfacing**  
**G. Ramsey**

Invitation to Bid 2016-001  
PTC 16.01 Street Resurfacing 2016

				Baldwin Paving	Blount Construction		CW Matthews		Stewart Brothers		
				Bidder 1	Bidder 2		Bidder 3		Bidder 4		
ITEM NO.	DESCRIPTION	UNIT	Total Quantity	Unit Cost	TOTAL	Unit Cost	TOTAL	Unit Cost	TOTAL	Unit Cost	TOTAL
100-1001	ADJUST MANHOLES & VALVES TO GRADE	EA	350	\$287.81	\$100,733.50	\$700.00	\$245,000.00	\$483.33	\$169,165.50	\$175.00	\$61,250.00
100-2001	GRADING COMPLETE	LS	1	\$81,724.57	\$81,724.57	\$70,000.00	\$70,000.00	\$493,163.00	\$493,163.00	\$54,500.00	\$54,500.00
402-1802	RECYCLED ASPH CONC PATCHING INCL BITUM MATL & H LIME	TN	14065	\$135.19	\$1,901,447.35	\$106.65	\$1,500,032.25	\$115.14	\$1,619,444.10	\$165.29	\$2,324,803.85
402-4510	RECYCLED ASPH CONC 9.5 MM SUPERPAVE, GP 2 ONLY, INCL BITUM MATL & H LIME (1.25")	TN	27,210	\$95.26	\$2,592,024.60	\$89.65	\$2,439,376.50	\$86.24	\$2,346,590.40	\$99.35	\$2,703,313.50
402-3190	RECYCLED ASPH CONC - D.Mix (.75")	TN	16,325	\$110.65	\$1,806,361.25	\$102.50	\$1,673,312.50	\$95.35	\$1,556,588.75	\$118.45	\$1,933,696.25
432-0206	MILLING ASPH CONC PVMT, VARIABLE DEPTH	SY	190,000	\$3.98	\$756,200.00	\$2.80	\$532,000.00	\$3.00	\$570,000.00	\$5.15	\$978,500.00
653-0120	THERMOPLASTIC PVMT MARKING, ARROW, TP 2	EA	1860	\$77.07	\$143,350.20	\$75.00	\$139,500.00	\$65.00	\$120,900.00	\$69.55	\$129,363.00
653-1501	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, WHITE	LF	62,000	\$0.59	\$36,580.00	\$0.60	\$37,200.00	\$0.50	\$31,000.00	\$0.55	\$34,100.00
653-1502	THERMOPLASTIC SOLID TRAF STRIPE, 5 IN, YELLOW	LF	84,000	\$0.71	\$59,640.00	\$0.70	\$58,800.00	\$0.60	\$50,400.00	\$0.65	\$54,600.00
653-1704	THERMOPLASTIC SOLID TRAF STRIPE, 24 IN, WHITE	LF	650	\$17.79	\$11,563.50	\$17.25	\$11,212.50	\$15.00	\$9,750.00	\$16.05	\$10,432.50
653-1804	THERMOPLASTIC SOLID TRAF STRIPE, 8 IN, WHITE	LF	1100	\$2.96	\$3,256.00	\$3.00	\$3,300.00	\$2.50	\$2,750.00	\$2.70	\$2,970.00
N/A	TRAFFIC SIGNAL LOOP (6"x50")	EA	10	\$1,183.34	\$11,833.40	\$1,750.00	\$17,500.00	\$925.00	\$9,250.00	\$1,550.00	\$15,500.00
<b>TOTAL BID:</b>					<b>\$7,504,714.37</b>	<b>TOTAL BID:</b>	<b>\$6,727,233.75</b>	<b>TOTAL BID:</b>	<b>\$6,979,001.75</b>	<b>TOTAL BID:</b>	<b>\$8,303,029.10</b>

Resurfacing Budget		Contract funds awarded	Balance available for award
FY16	\$1,500,000.00	\$678,073.50	\$821,926.50
FY17	\$1,500,000.00	\$0.00	\$1,500,000.00

<b>Total <u>Calendar</u> 2016 available =</b>
<b>\$2,321,926.50</b>

**Action Item**  
**Emergency Street**  
**Reconstruction**  
**Contract**  
**G. Ramsey**





This area to the left of the red line has settled to the point that a new low point has been established in the gutter. This area needs to be cut out, re-compacted, and then reconstructed (curb and street) to allow the gutter to drain to the south.





---

March 14, 2016

Mr. Gregory Ramsey, P.E.  
Director of Public Works  
147 Technology Parkway, Suite 200  
Peachtree Corners, Georgia 30092

Dear Mr. Gregory Ramsey,

We are pleased to submit the following proposal to address the settlement of pavement structure at 5698 Woodknoll Court. The scope of work includes the following

1. Demolition of 1,540 square feet – See attached plan view
2. Demolition of 72 linear feet of rollback curb
3. Design and Construction of 28 linear feet of retaining wall from 30 inches to 36 inches in height – See attached profile view
4. Construction of 44 linear feet of header curb
5. Installation of GAB to raise existing pavement grades
6. Installation of 2 inches of 19 mm asphalt and 1 inch of 9.5 mm asphalt

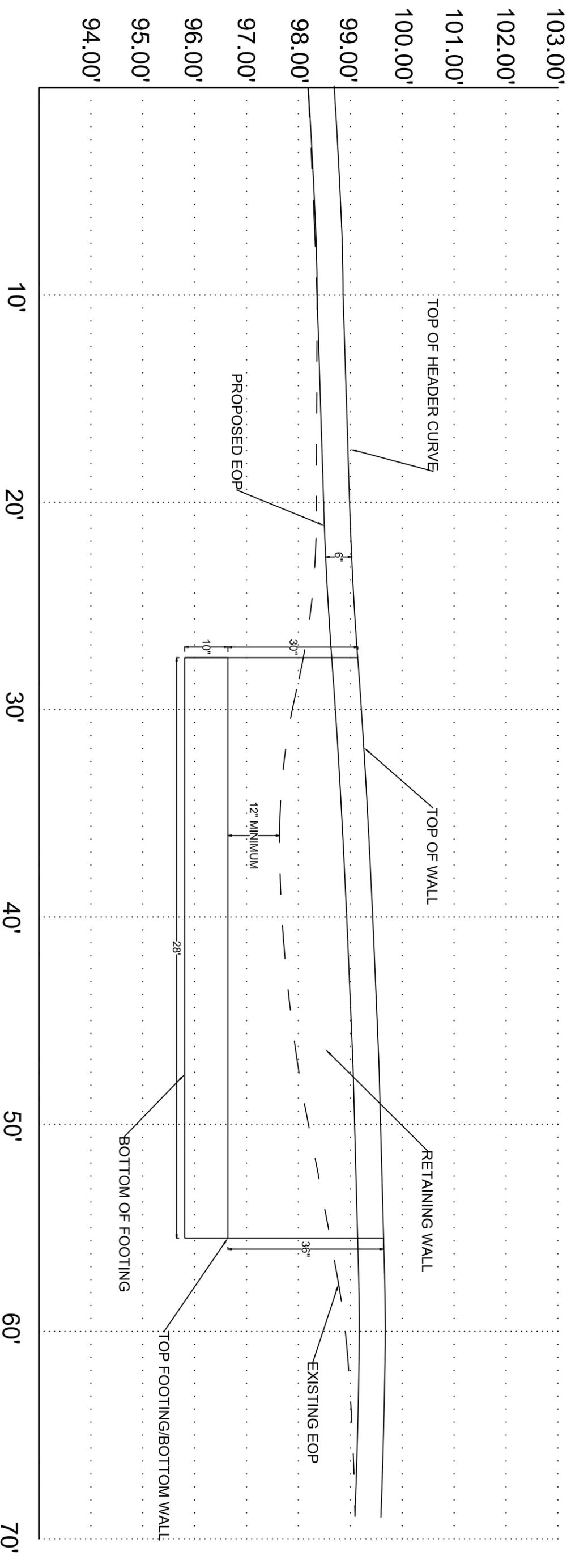
The fee is \$28,900.

Warm Regards,

A handwritten signature in blue ink, appearing to read "Luigi Hernandez", is written over a light blue horizontal line.

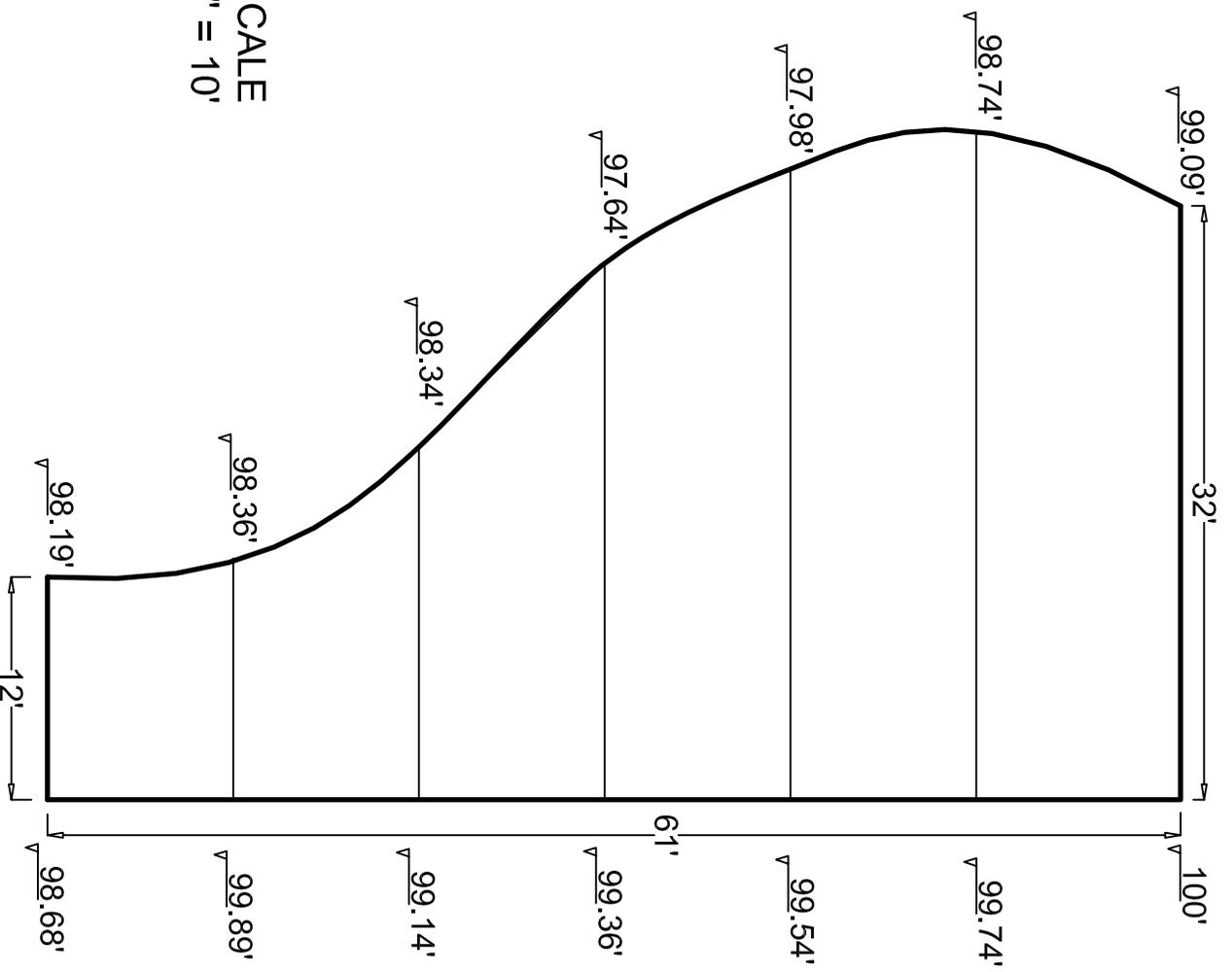
Luigi Hernandez  
President

# PROFILE - EDGE OF PAVEMENT (FACE OF CURB)



SCALE  
H: 1" = 5'  
V: 1" = 2'

PLAN VIEW



SCALE  
1" = 10'

**Work Session  
Crosswalk Study  
Report  
G. Ramsey**



**PEDESTRIAN CROSSING STUDY  
WINTERS CHAPEL ROAD AT  
WINTERBROOK COURT/WINTERGREEN COURT**

CITY OF PEACHTREE CORNERS  
GWINNETT COUNTY, GEORGIA

W & A Project No. 16-TF-006  
CIP PROJECT #PTC 15.17

March 31<sup>th</sup>, 2016

Prepared By:

Wolverton & Associates, Inc.  
6745 Sugarloaf Parkway, Suite 100  
Duluth, GA 30097  
770-447-8999 (Tel)  
770-447-9070 (Fax)

Mr. Todd DeVos, P.E., PTOE  
Todd.devos@Wolverton-assoc.com

Prepared For:

Mr. Greg Ramsey, P.E.  
Director of Public Works  
City of Peachtree Corners  
147 Technology Parkway, Suite 200  
Peachtree Corners, GA 30092

WOLVERTON & ASSOCIATES, INC.  
6745 SUGARLOAF PARKWAY  
SUITE 100  
DULUTH, GA 30097  
(770) 447-8999 PHONE  
(770) 447-9070 FAX  
www.wolverton-assoc.com

# TABLE OF CONTENTS

---

TABLE OF CONTENTS .....	i
LIST OF FIGURES.....	ii
LIST OF TABLES .....	ii
<b>1. INTRODUCTION .....</b>	<b>1</b>
Methodology.....	1
<b>2. EXISTING CONDITIONS.....</b>	<b>2</b>
Study Area Characteristics.....	2
Existing Traffic Flow Data.....	4
<b>3. POTENTIAL SOLUTIONS/ANALYSIS .....</b>	<b>6</b>
<b>4. SUMMARY AND RECOMMENDATIONS .....</b>	<b>13</b>
<b>5. REFERENCES .....</b>	<b>15</b>
<b>6. APPENDICES .....</b>	<b>16</b>

# LIST OF FIGURES

---

Figure 1 – Study Area Map .....	1
Figure 2 – Study Area Existing Travel Lanes and Traffic Control .....	2
Figure 3 – Existing Traffic Flow Data & Observed Pedestrian Crossing Locations.....	5
Figure 4 – Example of Pedestrian Crossing with No Flashers .....	6
Figure 5 – Example of Pedestrian Crossing with .....	7
Figure 6 – Example of Pedestrian Crossing with Wig Wag Flashers Active by Push Button .....	8
Figure 7 – Example of Pedestrian Crossing with Pedestrian Activated Rapid Flashing Beacon .....	9
Figure 8 – Example of Pedestrian Crossing with Hawk Signal .....	10
Figure 9 – Guidelines for Installing Hawk Signal (High Speed Roadways).....	11
Figure 10 – Warrant 4 (70% Factor), Traffic Signal Warrants Analysis.....	12
Figure 11 – Proposed Hawk Signal Crosswalk Location .....	14

# LIST OF TABLES

---

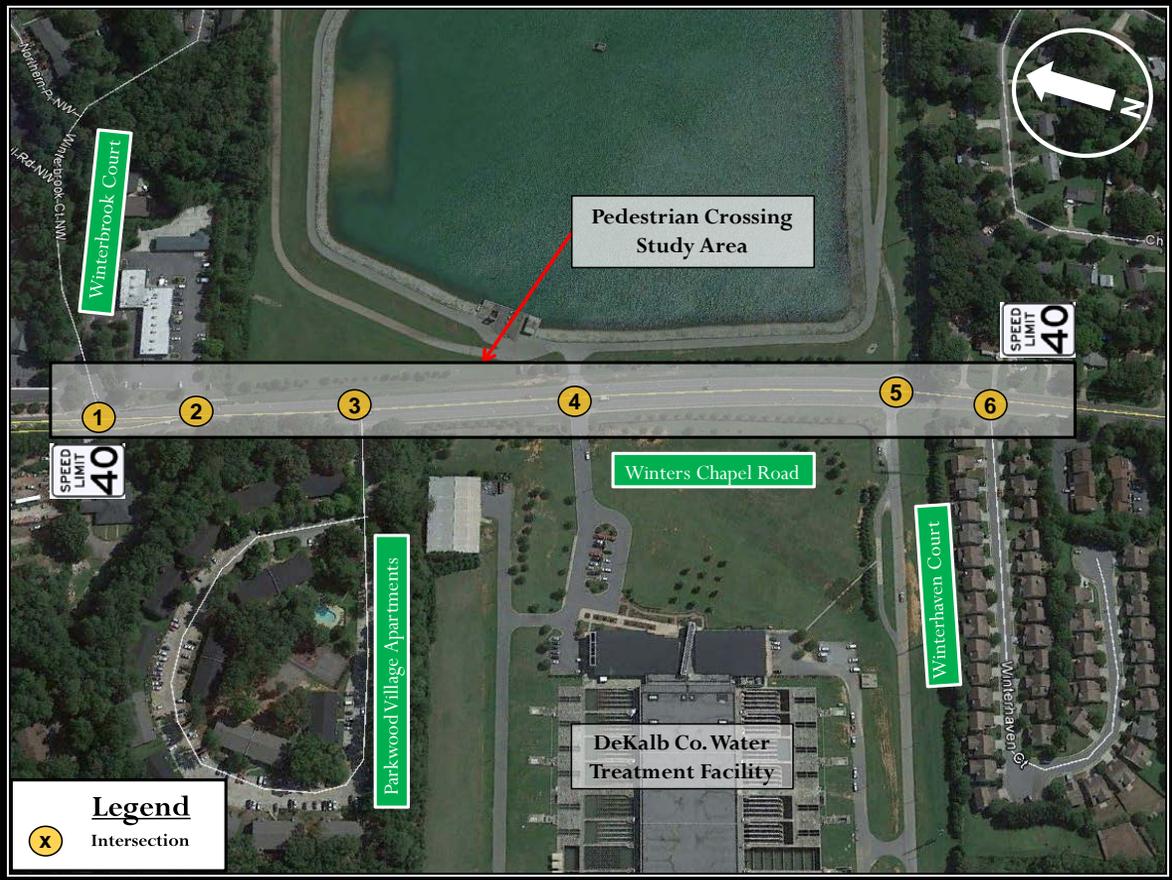
Table 1 – Pedestrian Data Collection Results .....	4
--	---

# 1. INTRODUCTION

## PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

This pedestrian crossing study was conducted to evaluate the pedestrian activity crossing Winters Chapel Road in the vicinity of Winterbrook Court and Winterhaven Court. This study analyzes the various options that can be utilized to provide a safer method of crossing Winters Chapel Road as well as the best location for the crossing within the study areas project limits. The study area is shown in Figure 1.

Figure 1 – Study Area Map



### Methodology

Initial evaluations were made of the various types of pedestrian crossings utilized in Georgia. The Georgia Department of Transportation (GDOT) and the Manual on Uniform Traffic Control Devices (MUTCD) specify criteria for deciding where, if, and the type of marked crosswalk that should be installed. The applicable criteria required to warrant approved types of pedestrian crossings was gathered and was used in identifying the necessary data. The collected data included Pedestrian count data, vehicular count data, and vehicular speed data. All data were collected on typical weekdays with good weather to provide accurate count data for the analysis. The study area was then analyzed to determine if a need exists for installing a crosswalk treatment and then if so, the best location and type of crossing for the area.

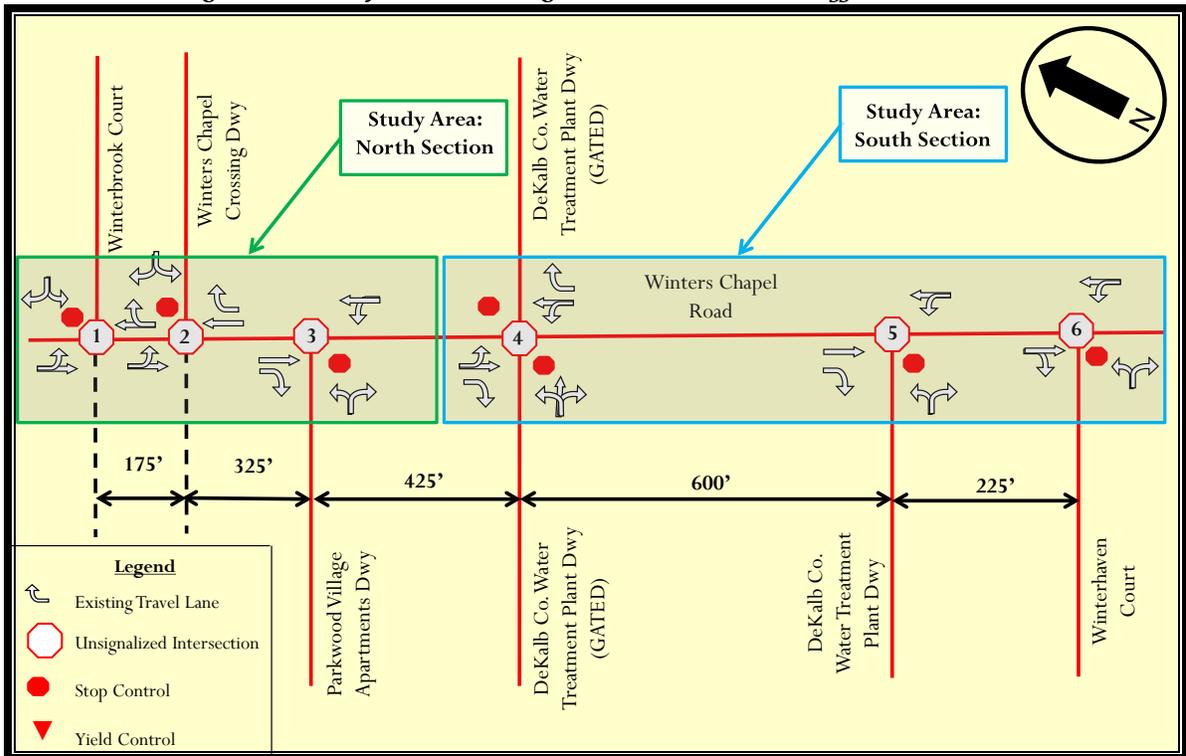
## 2. EXISTING CONDITIONS

### PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

#### Study Area Characteristics

The study area along Winters Chapel Road is located between Winterbrook Court to the north and Winterhaven Court to the south. This encompasses about a third of a mile section of Winters Chapel Road located in the southwest portion of the City of Peachtree Corners and directly along the County line between Gwinnett County and DeKalb County. Winters Chapel Road is a two lane roadway in the vicinity of the study area. The speed limit on Winters Chapel Road is posted at 40 mph. The study area encompasses three intersections as well as the two DeKalb County Water Treatment Facility driveways, of which one is gated. The existing lane geometry and traffic control for the three intersections located inside the study area are shown in Figure 2.

Figure 2 – Study Area Existing Travel Lanes and Traffic Control



Winterbrook Court (Intersection #1) serves as a single access point to three residential housing developments, and forms a three-legged intersection with Winters Chapel Road. Winters Chapel Crossing (Intersection #2) is a commercial development that forms a T-shaped intersection with Winters Chapel Road, and is located approximately 175 feet south of Winterbrook Court. Parkwood Village Apartments driveway (Intersection #3) is a residential apartment complex driveway that forms a T-shaped intersection with Winters Chapel Road, and is located approximately 325 feet south of the Winters Chapel Crossing driveway.

The DeKalb County Water Treatment Plant has two driveways that intersect with Winters Chapel Road. The first driveway (Intersection #4) is located 425 feet to the south of the Parkwood Village Apartments Driveway. The first driveway forms a four-legged intersection with Winters Chapel Road, with both eastbound and westbound approaches being gated.

The second driveway (Intersection #5) is located approximately 600 feet to the south of the first driveway. The second driveway forms a T-shaped intersection with Winters Chapel Road. Winterhaven Court (Intersection #6) serves as a single access point to residential homes and forms a T-shaped intersection with Winters Chapel Road. Winterhaven Court is located approximately 225 feet south of the southernmost DeKalb County Water Treatment Plant driveway. All of the intersections defined within the study area are Stop Controlled intersections with the side street approaches being stop controlled and the main street (Winters Chapel Road) being free flow.

There are sidewalks located along both the east and west sides of Winters Chapel Road inside the study area. There are currently no marked crosswalks in the study area. The nearest crosswalk to cross Winters Chapel Road is located approximately 575 feet to the north at the signalized intersection of Winters Chapel Road and Peeler Road/Shopping Center Driveway. Although this signal provides pedestrian access across Winters Chapel Road, the intersection is out of the pedestrian pathway and located out of the study area. Therefore, the signal is not be a feasible crossing location for pedestrians inside the study area.

If a pedestrian facility in the study area is warranted, it needs to be located close to where the pedestrians are currently crossing Winters Chapel Road. Since the study area covers a large area, pedestrian activity was observed on two different days. One day observed pedestrian crossing activity in the southern section of the study area, and the second day observed pedestrian crossing activity in the northern section of the study area. Data collection activities showed marginal pedestrian crossing activity in the southern section during both AM and PM peak periods, but significant pedestrian crossing activity in the northern section during both AM and PM peak periods.

**Existing Traffic Flow Data**

24-hour Vehicle tube count and vehicle speed data were collected on Wednesday, March 9<sup>th</sup>, 2016 inside the study area. Additionally, the number of pedestrians that entered the study area was counted, as well as the number of pedestrians who crossed East Jones Bridge Road within the study area. Pedestrian counts were taken during the AM Peak period (7:00 AM to 9:00 AM) and during the PM Peak period (4:00 PM to 6:00 PM). Pedestrian counts for the eastern section of the study area were collected on Wednesday, March 2<sup>nd</sup>, 2016 for the AM and PM Peak periods. Pedestrian counts for the western section of the study area were collected on Thursday, March 3<sup>rd</sup>, 2016 for the AM peak period and on Tuesday, March 8<sup>th</sup>, 2016 for the PM Peak period. Table 1 shows the number of pedestrians that crossed Winters Chapel Road as well as the number of pedestrians that did not cross Winters Chapel Road. The pedestrians that did cross were observed to cross Winters Chapel Road in specific locations. Weather conditions during the AM peak period data collection on March 2<sup>nd</sup> and March 3<sup>rd</sup> was sunny and 40 degrees. Weather conditions during the PM peak period data collection on March 2<sup>nd</sup> and March 8<sup>th</sup> was sunny & 50 degrees and sunny & 73 degrees, respectively.

**Table 1 – Pedestrian Data Collection Results**

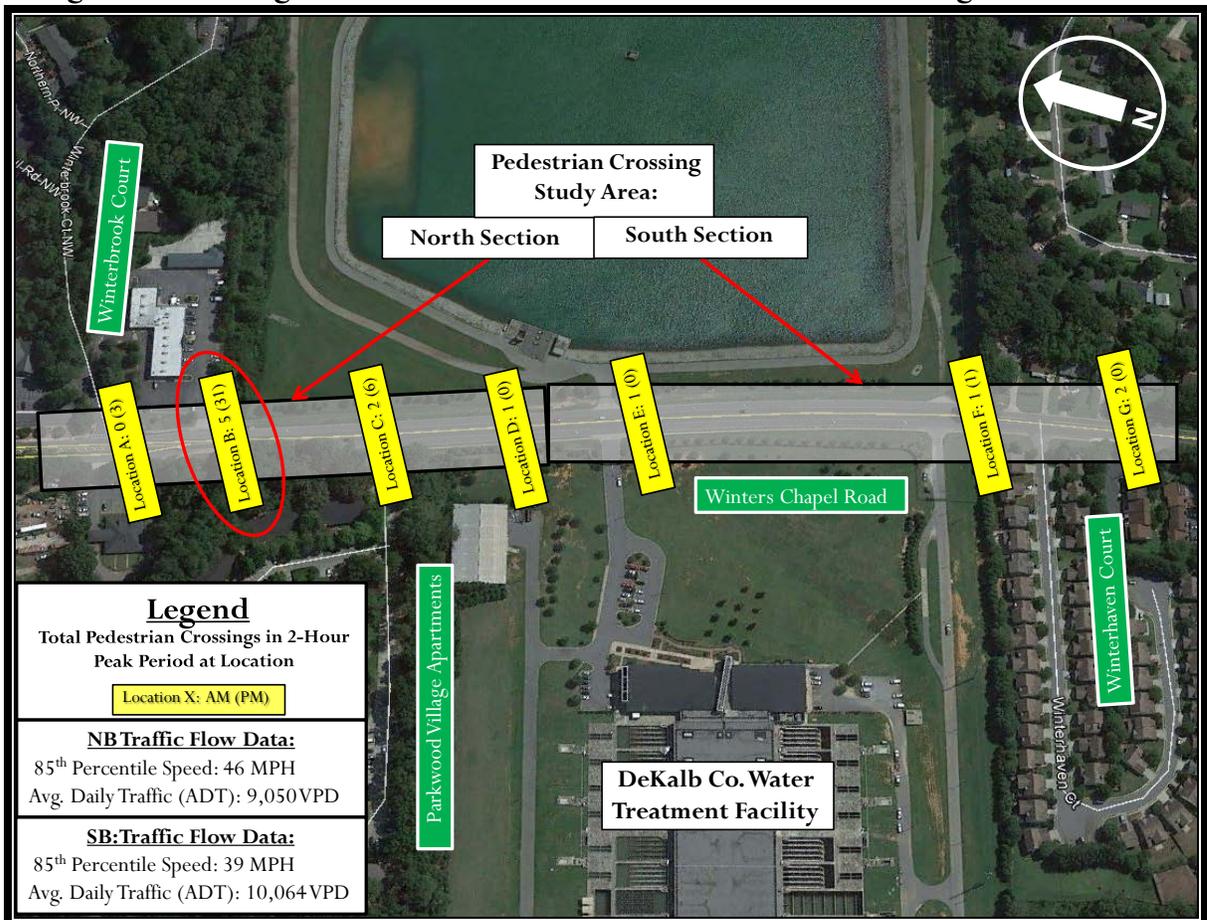
Location #2: Winters Chapel Road (South Section) Between Winterhaven Ct & Winterbrook Ct			Location #2: Winters Chapel Road (South Section) Between Winterhaven Ct & Winterbrook Ct		
Observation Dates: AM & PM - 3/2/16			Observation Dates: AM & PM - 3/2/16		
Time Period	PEDs Parallel to Winters Chapel Rd (Not Crossing)	PEDs Crossing Winters Chapel Rd (South Section)	Time Period	PEDs Parallel to Winters Chapel Rd (Not Crossing)	PEDs Crossing Winters Chapel Rd (South Section)
	AM PEAK			AM PEAK	
7:00 AM - 7:30 AM	2	0	7:00 AM - 7:30 AM	2	0
7:30 AM - 8:00 AM	2	2	7:30 AM - 8:00 AM	2	2
8:00 AM - 8:30 AM	1	2	8:00 AM - 8:30 AM	1	2
8:30 AM - 9:00 AM	2	0	8:30 AM - 9:00 AM	2	0
Total	7	4	Total	7	4
PM PEAK			PM PEAK		
4:00 PM - 4:30 PM	5	0	4:00 PM - 4:30 PM	5	0
4:30 PM - 5:00 PM	6	0	4:30 PM - 5:00 PM	6	0
5:00 PM - 5:30 PM	7	1	5:00 PM - 5:30 PM	7	1
5:30 PM - 6:00 PM	6	0	5:30 PM - 6:00 PM	6	0
Total	24	1	Total	24	1

The pedestrians that did not cross Winters Chapel Road did not influence the crossing type of location, but are shown for informational purposes to show that additional pedestrian activity is present along the corridor. There were a total of seven (7) observed crossing locations within the study area.

Figure 3 shows a breakdown of where along the corridor the pedestrian crossing activity occurred. Location A is located at the intersection of Winters Chapel Road and Winterbrook Court. Location B is located approximately 275 feet north of the Parkwood Village Apartment Complex Driveway at a wooded unpaved path that connects to the Parkwood Village Apartment Complex. Location C is located at the intersection of Winters Chapel Road and the Parkwood Village Apartment Complex Driveway. Location D is located approximately 125 feet to the north of the DeKalb County Water Treatment Plant northern (Gated) driveway. Location E is located approximately 50 feet to the south of the DeKalb County Water Treatment Plant northern (Gated) driveway. Location F is located approximately 50 feet south of the DeKalb County Water Treatment Plant southern (Gated) driveway. Finally, Location G is located at the intersection of Winters Chapel Road and Winterhaven Court.

As can be seen in Figure 3, the majority of the pedestrians crossing Winters Chapel Road occurred at Location B in both the AM and PM Peak Hours. Most of the pedestrians were traveling to/from the Parkwood Village Apartments to/from the Winters Chapel Crossing strip mall. Existing traffic flow data, including the 85<sup>th</sup> percentile speed and average daily traffic per direction, is summarized in Figure 3.

**Figure 3 – Existing Traffic Flow Data & Observed Pedestrian Crossing Locations**



### 3. POTENTIAL SOLUTIONS/ANALYSIS

#### PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

There are various types of pedestrian crossings that can be utilized to allow for a safer pedestrian crossing than what currently exists in the study area today. The following is a list of the various types of crossing and descriptions of their operation. While GDOT’s approval of a pedestrian crossing treatment installation, if applicable, is not required since Winters Chapel Road is not a designated state route, this report still uses GDOT’s criteria for installing a pedestrian crossing treatment. These various types of pedestrian crossings vary in amount of warning for the vehicle, the indications for the vehicle and pedestrian, and the method of activation.

##### *Treatment #1: Pedestrian Sign with No Flashing Beacons*

The pedestrian sign with no flashing beacons is comprised of a crosswalk with a static pedestrian sign (W11-2). This type of crossing advises the driver of potential pedestrian activity in the vicinity: It provides identification of a crosswalk, but does not give drivers a different message when there is a pedestrian present at the crosswalk. This type of crossing is utilized in lower pedestrian volume areas and sections of roadway that have no sight distance restrictions, whereby providing drivers with a clear line of sight to the crosswalk. It should be noted that there are no required pedestrian volumes or vehicular speeds and volumes to warrant this type of crossing treatment. However, this type of treatment is typically used in low pedestrian volume locations. The maximum pedestrian crossing volume at any hour of the day is 26. Therefore, this crossing treatment should not be the first installation choice. Figure 4 shows the intersection of Peeler Road and Lakeside Drive in Dunwoody, which is an example of a pedestrian crossing with no flashing beacons. This location is less than one (1) mile from the study area.

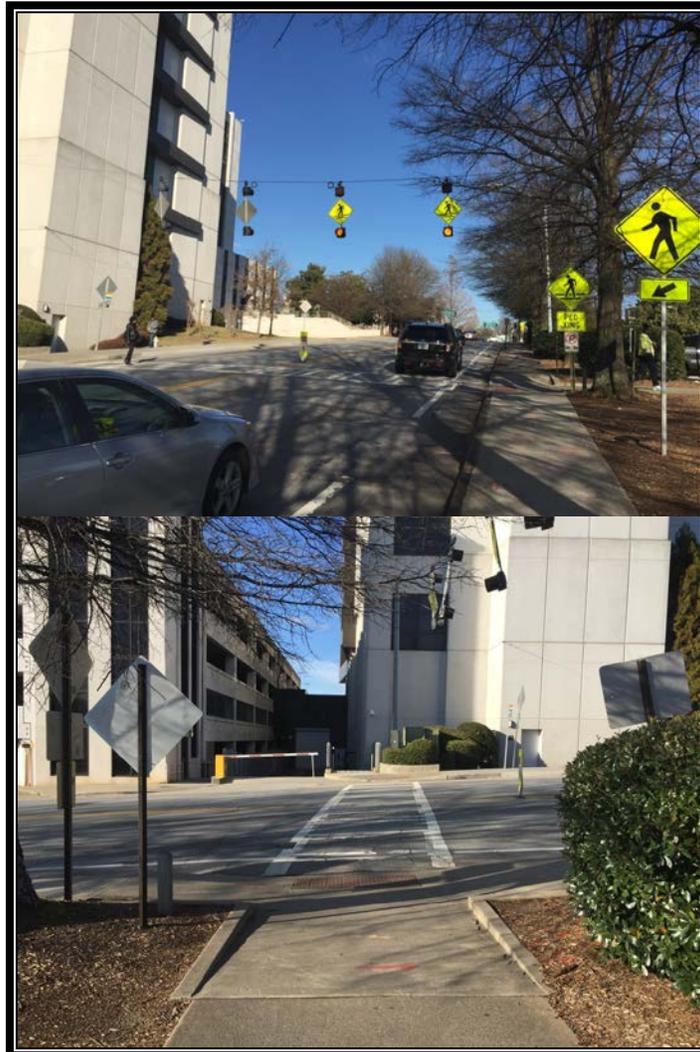
*Figure 4 – Example of Pedestrian Crossing with No Flashers*



***Treatment #2: Pedestrian Sign with Wig Wag Flashers***

The pedestrian sign with wig wag flasher type crossing is comprised of a crosswalk with a pedestrian sign (W11-2) that includes wig wag flashers on both vehicular approaches. The wig wag flashers operate continuously (24-hours a day) and thus are not activated by the presence of a pedestrian. No pedestrian button is present at this type of crossing. This type of crossing warns the driver of a potential pedestrian crossing but does not operate any differently with a pedestrian present or absent. This type of crossing is utilized in lower pedestrian volume areas and provides a warning to the driver that pedestrian activity is common in the area. However, there are no specific pedestrian volumes or vehicular volume and speed criteria to warrant this type of crossing treatment. The maximum pedestrian crossing volume at any hour of the day is 26. This type of pedestrian treatment could be utilized, but should not be the first installation choice. Figure 5 shows an example of a midblock pedestrian crossing in downtown Decatur, which has continuous wig wag flashers.

***Figure 5 – Example of Pedestrian Crossing with Continuous Wig Wag Flashers***



***Treatment #3: Pedestrian Sign with Wig Wag Flashers Activated by Pedestrian***

The pedestrian sign with wig wag flasher type crossing is comprised of a crosswalk with a pedestrian sign (W11-2) that includes wig wag flashers on both vehicular approaches. The wig wag flashers operate for a short duration of time only when there is the presence of a pedestrian. The flashers are activated by a pedestrian push button, which is present at this type of crossing. This type of crossing warns the driver of a pedestrian crossing at this location. This type of crossing is utilized in lower pedestrian volume areas and provides a warning to the driver that pedestrian activity is common in the area. The pushbutton activation feature warns a driver that a pedestrian is either waiting to cross, in the process of crossing, or has just finished crossing. This type of installation is no longer utilized, and has been replaced with Treatment #4. Figure 6 shows the midblock pedestrian crossing on Peachtree Corners Circle located just northeast of the intersection of with Eastman Trail in Peachtree Corners. Treatment #4, which consists of a Pedestrian sign with Rapid Flashing Beacon (RFB), is described next.

***Figure 6 – Example of Pedestrian Crossing with Wig Wag Flashers Active by Push Button***



***Treatment #4: Pedestrian Sign with Rapid Flashing Beacon (RFB)***

The pedestrian sign with RFB is comprised of a crosswalk with a pedestrian sign (W11-2) that includes an RFB. The beacon is activated by a pedestrian button located at each end of the crosswalk. The RFB remains dark until activated by the button. A center refuge island can be located in the center of the roadway to shorten the crossing distance and provide a safer crossing. No indications are present for the pedestrian. This type of crossing can be used in lower to moderate pedestrian volume areas and provides a warning to the driver when a pedestrian is present. However, there are no specific pedestrian volumes or vehicular volume and speed criteria to warrant this type of crossing treatment. Figure 7 shows the midblock crossing with an RFB on Tilly Mill Road at the city limit of the city of Dunwoody.

***Figure 7 – Example of Pedestrian Crossing with Pedestrian Activated Rapid Flashing Beacon***



### *Treatment #5: Hawk Signal*

A Hawk signal is a High intensity Activated cross Walk. Depending upon the width of the roadway and the median size, the crossing can be a one or two stage crossing. A pedestrian utilizing the crossing presses the button to activate the first signal, when the traffic signal turns red, a “walk” indication allows the pedestrian to cross the first stage. Once reaching the center refuge island the pedestrian presses the second button to activate the second signal. The vehicular indication is a three section signal head with two side by side red indications above a yellow indication. The vehicular signals are dark until activated by the pedestrian, while the pedestrian signals are a solid orange hand (don’t walk) indication. Once activated by the pedestrian, the vehicular signal flashes yellow to warn drivers that the signal has been activated. The vehicular signal then turns to solid yellow to prepare the drivers to stop. Both of the red vehicular indications turn solid red while the pedestrian signal changes to a white walking man. After the walk time has elapsed the pedestrian clearance times out, the pedestrian signal changes to a flashing orange hand and a countdown timer, while the vehicular indication changes to a flashing red. The flashing red allows a vehicle to proceed through the crosswalk after the pedestrian has crossed its path. After the countdown time has elapsed, the signals revert back to solid orange hand for the pedestrian and dark for the vehicle. Figure 8 shows the midblock signalized crossing along SR 155/Candler Road in Decatur, which is an example of a pedestrian crossing with a hawk signal.

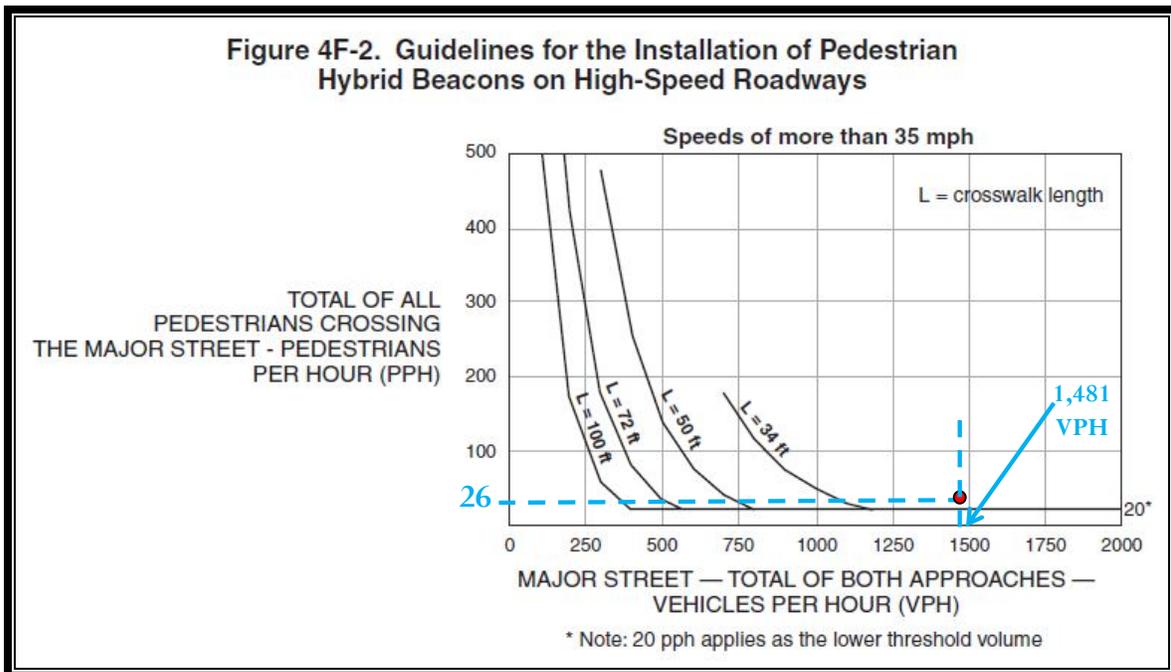
*Figure 8 – Example of Pedestrian Crossing with Hawk Signal*



Guidelines for the installation of pedestrian signals on high-speed roadways (speeds greater than 35 MPH) were applied from the MUTCD for the observed crossing locations within the study area, and are shown below. The posted speed limit along this section of Winters Chapel Road is 40 MPH and the overall 85<sup>th</sup> Percentile speed was measured to be 43 MPH. Therefore, Figure 9 is utilized for determining if a Hawk signal is warranted at a particular location. Each curve has an associated expected crosswalk length (L), if a marked crosswalk were to be installed. Winters Chapel Road is a two lane two way roadway. The location with the highest pedestrian crossing activity within the study area occurred between the Parkwood Village Apartments Driveway and a strip mall driveway, located approximately 300 feet to the north. This is Location D, as marked in Figure 3. At this crossing location, the approximate curb to curb crossing distance is approximately 40 feet. The highest one hour peak pedestrian crossings occurred between 4:00 PM and 5:00 PM inside the study area, and totaled twenty-six (26) crossings. The hour bi-directional volume between 4:00 PM and 5:00 PM is 1,481 vehicles per hour.

As can be seen, the combination of the major street total bi-directional traffic and the number of pedestrians crossing in the peak hour is above the applicable curve. Since the number of pedestrian crossings during the peak hour is above the minimum threshold of 20 pedestrian crossings per hour, the warrant for installing a Hawk Signal IS met for installation.

Figure 9 – Guidelines for Installing Hawk Signal (High Speed Roadways)



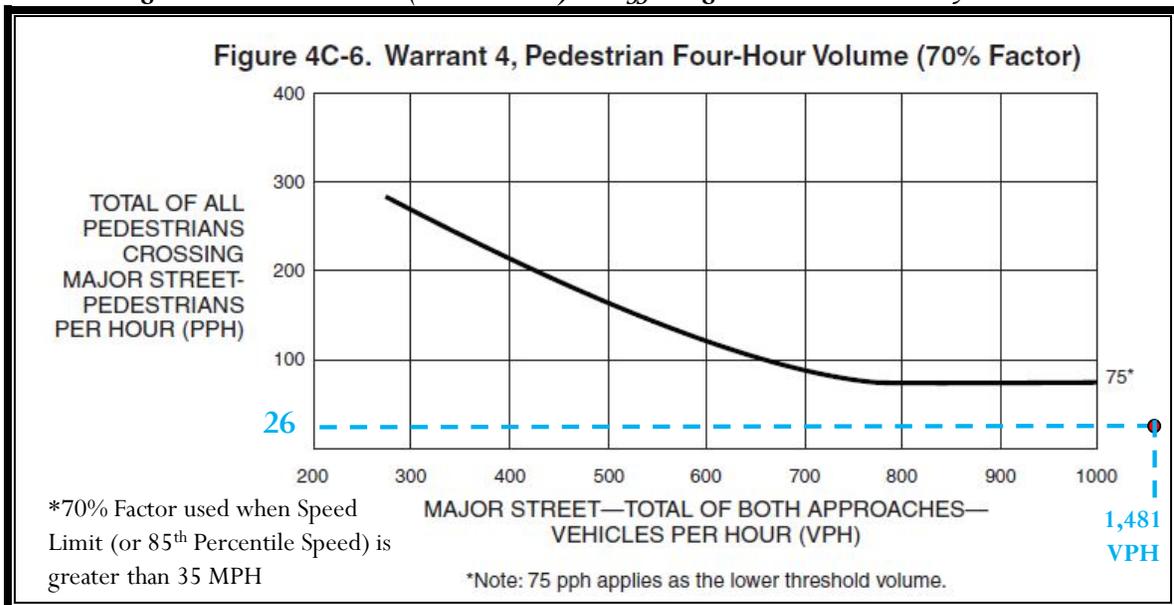
### Treatment #6: Traffic Signal

The traffic signal provides the highest level of pedestrian crossing protection across all approaches to an intersection that are signalized. Both vehicles and pedestrians each have their own signal indication. A pedestrian utilizing a crossing at a signalized intersection presses the push button to place a call into the signal controller. Depending upon the programming of the signal at the intersection, the pedestrian is given a “walk” indication either at the same time as the adjacent vehicular movement (e.g. concurrent pedestrian phase) or by itself (e.g. exclusive pedestrian phase). After the “walk” indication is given, a flashing hand with or without a countdown of time remaining to cross before right of way is given to another direction and vehicular movement.

Warrant 4 in the traffic signal warrants section of the MUTCD is shown below. It should be noted that the 70% pedestrian volume can be used since the measured 85<sup>th</sup> percentile speed is greater than 35 MPH. Figure 10 shows the results of the pedestrian crossing warrant. The highest pedestrian one hour peak pedestrian crossings occurred between 4:00 PM and 5:00 PM inside the study area, and totaled twenty-six (26) crossings. The PM peak hour bi-directional volume occurred between 4:00 PM and 5:00 PM is 1,481 vehicles per hour.

As can be seen, the major street total bi-directional traffic during the peak hour and the number of pedestrians crossing in the peak hour fall below the curve. Since the number of pedestrian crossings during the peak hour falls below the minimum threshold of 75 pedestrian crossings per hour, Warrant 4 is NOT met for the installation of a standard traffic signal.

Figure 10 – Warrant 4 (70% Factor), Traffic Signal Warrants Analysis



## 4. SUMMARY AND RECOMMENDATIONS

---

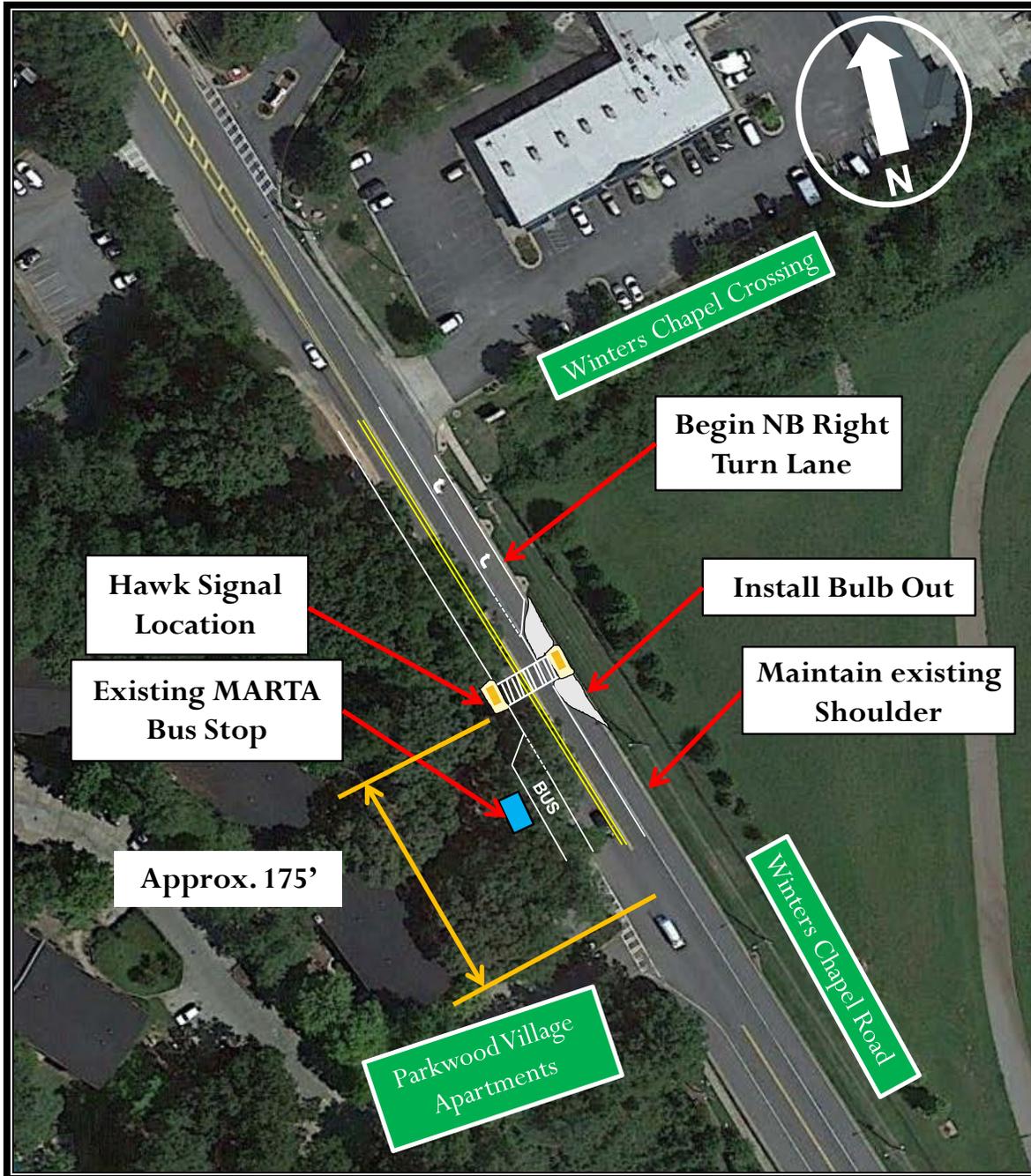
### PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

An analysis of crash data revealed no pedestrian related accidents or fatalities within the study area. Stopping sight distance appears adequate at all seven (7) of the observed crossing locations within the study area. Based upon the analyses of peak hour data, the installation of a traffic signal (Treatment #6) is not warranted. However, the installation of a Hawk Signal (Treatment #5) is warranted and should be highly considered for installation. Since Treatment #3 is no longer installed, the available remaining options are to install Treatments #1, #2, and #4. A standalone crosswalk without any beacons (Treatment #1) could be installed, but more than likely would not be an effective installation due to heavy mainline traffic flow and high pedestrian volume. Installing Treatment #2 may not be effective given the combination of low typical pedestrian crossing activity and the continuous wig-wag flashers, whereby desensitizing drivers to the presence of a pedestrian. An RFB (Treatment #4) can be considered but may not be an effective installation, due to the combination of a high vehicular traffic volume on Winters Chapel Road with high pedestrian volumes during the highest one hour peak pedestrian crossing period.

It is recommended that a Hawk Signal (Treatment #5) be installed at Location B within the study area. The MUTCD recommends the placement of any type of midblock crosswalk to be at least 100 feet away from driveways that are signal, stop, or yield controlled. In addition, GDOT does not allow Hawk Signals to be placed within 300 feet of an existing marked crosswalk or signalized intersection. The optimal location for the installation of the Hawk Signal is as close to Location B as possible. The Hawk Signal would in most cases funnel pedestrians from the other observed crossing locations to the signal's proposed location. The proposed location is approximately 175 feet north of the Parkwood Village Apartment Complex Driveway. Installing bulb outs on the east side of the crosswalk is recommended to prevent vehicles wanting to turn right into the Winters Chapel Crossing Driveway from driving through the crosswalk in the shoulder lane. The bulb out on the northeast side of the crosswalk would also act as a taper from the shoulder to a right turn lane into Winters Chapel Crossing.

This location is north of an existing MARTA bus stop, since the MARTA bus stop is located in the right turn lane into the Parkwood Village Apartments, the stopped MARTA bus will not block the visibility of pedestrians and northbound vehicles at the proposed pedestrian crossing location. Figure 11 illustrates the proposed location.

Figure 11 – Proposed Hawk Signal Crosswalk Location



## 5. REFERENCES

---

### PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

1. AASHTO A Policy on Geometric Design of Highways and Streets, 6<sup>th</sup> Edition, American Association of State Highway and Transportation Officials, Washington, DC, 2011.
2. Manual on Uniform Traffic Control Devices, 2009 Edition, Federal Highway Administration, Washington, DC, 2009.
3. Traffic Calming: State of the Practice, Chapter 3 – Toolbox of Traffic Calming Measures. Institute of Transportation Engineers (ITE), August 1999.
4. “Safety Effects of Marked Versus Unmarked Crosswalks at Uncontrolled Locations: Final Report and Recommended Guidelines.” Research, Development, and Technology Turner-Fairbank Highway Research Center. U.S. Department of Transportation Federal Highway Administration. McLean, VA, 2005.

## 6. APPENDICES

---

### PEDESTRIAN CROSSING STUDY – WINTERS CHAPEL ROAD AT WINTERBROOK COURT/WINTERHAVEN COURT

#### APPENDIX A: TRAFFIC COUNTS

# Greater Traffic Company

Winters Chapel Rd just Northwest  
of Dekalb County Water plant gate.

nb

Start Time	0 15	16 20	21 25	26 30	31 35	36 40	41 45	46 50	51 55	56 60	61 65	66 70	71 75	76 9999	Total	Pace Speed	Number in Pace
03/09/16	2	0	0	0	3	9	20	8	3	0	0	0	0	0	45	36-45	29
01:00	0	0	0	0	3	6	11	6	2	0	0	0	0	0	28	36-45	17
02:00	0	0	0	0	1	5	12	3	0	0	0	0	0	0	21	36-45	17
03:00	0	0	0	0	3	4	6	7	1	0	0	0	0	0	21	41-50	13
04:00	0	0	0	0	0	4	11	7	0	0	0	0	0	0	22	41-50	18
05:00	0	0	0	1	4	9	23	22	4	0	0	0	0	0	63	41-50	45
06:00	4	3	11	13	27	52	96	60	18	0	0	0	0	0	284	41-50	156
07:00	11	10	24	52	75	128	161	109	29	0	0	0	0	0	599	36-45	289
08:00	4	1	7	31	139	202	231	123	16	0	0	0	0	0	754	36-45	433
09:00	0	4	3	10	32	112	286	130	27	0	0	0	0	0	604	41-50	416
10:00	0	1	3	3	14	33	140	123	23	0	0	0	0	0	340	41-50	263
11:00	0	0	0	4	18	37	149	116	39	0	0	0	0	0	363	41-50	265
12 PM	4	6	7	19	24	39	137	131	43	0	0	0	0	0	410	41-50	268
13:00	1	1	4	15	50	90	175	96	15	0	0	0	0	0	447	41-50	271
14:00	7	5	8	32	43	96	187	88	20	0	0	0	0	0	486	36-45	283
15:00	5	1	5	15	51	167	245	89	16	0	0	0	0	0	594	36-45	412
16:00	17	14	27	31	112	183	195	90	17	0	0	0	0	0	686	36-45	379
17:00	6	12	20	47	165	288	272	74	10	0	0	0	0	0	894	36-45	560
18:00	5	10	27	77	204	269	149	41	8	0	0	0	0	0	790	31-40	473
19:00	4	5	28	62	97	178	135	26	5	0	0	0	0	0	540	36-45	313
20:00	0	0	1	16	76	133	144	25	8	0	0	0	0	0	403	36-45	277
21:00	0	1	3	11	38	127	113	41	5	0	0	0	0	0	339	36-45	240
22:00	0	1	3	14	19	39	86	30	9	0	0	0	0	0	201	36-45	125
23:00	0	0	0	0	13	22	44	32	4	1	0	0	0	0	116	41-50	76
<b>Total</b>	<b>70</b>	<b>75</b>	<b>181</b>	<b>453</b>	<b>1211</b>	<b>2232</b>	<b>3028</b>	<b>1477</b>	<b>322</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9050</b>		
Percent	0.8%	0.8%	2.0%	5.0%	13.4%	24.7%	33.5%	16.3%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	07:00	07:00	07:00	07:00	08:00	08:00	09:00	09:00	11:00								08:00
Vol.	11	10	24	52	139	202	286	130	39								754
PM Peak	16:00	16:00	19:00	18:00	18:00	17:00	17:00	12:00	12:00	23:00							17:00
Vol.	17	14	28	77	204	288	272	131	43	1							894
<b>Total</b>	<b>70</b>	<b>75</b>	<b>181</b>	<b>453</b>	<b>1211</b>	<b>2232</b>	<b>3028</b>	<b>1477</b>	<b>322</b>	<b>1</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>9050</b>		
Percent	0.8%	0.8%	2.0%	5.0%	13.4%	24.7%	33.5%	16.3%	3.6%	0.0%	0.0%	0.0%	0.0%	0.0%			

15th Percentile : 32 MPH  
 50th Percentile : 40 MPH  
 85th Percentile : 46 MPH  
 95th Percentile : 49 MPH

Stats  
 10 MPH Pace Speed : 36-45 MPH  
 Number in Pace : 5263  
 Percent in Pace : 58.2%  
 Number of Vehicles > 40 MPH : 4828  
 Percent of Vehicles > 40 MPH : 53.3%  
 Mean Speed(Average) : 40 MPH

# Greater Traffic Company

Winters Chapel Rd just Northwest  
of Dekalb County Water plant gate.

sb

Start Time	0	16	21	26	31	36	41	46	51	56	61	66	71	76	9999	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75					
03/09/16	0	0	2	1	14	20	9	3	0	0	0	0	0	0	0	49	31-40	34
01:00	0	0	2	1	6	11	7	2	1	0	0	0	0	0	0	30	34-43	18
02:00	0	0	0	1	5	11	3	1	0	0	0	0	0	0	0	21	31-40	16
03:00	0	0	1	0	3	6	2	2	0	0	0	0	0	0	0	14	31-40	9
04:00	0	1	0	2	9	15	6	0	0	0	0	0	0	0	0	33	31-40	24
05:00	1	1	7	6	9	35	23	4	1	0	0	0	0	0	0	87	36-45	58
06:00	1	4	35	43	66	142	112	9	1	0	0	0	0	0	0	413	36-45	254
07:00	6	13	70	128	238	283	110	3	0	1	0	0	0	0	0	852	31-40	521
08:00	46	40	49	82	250	262	89	3	0	0	0	0	0	0	0	821	31-40	514
09:00	0	0	8	16	100	280	153	20	0	0	0	0	0	0	0	577	36-45	433
10:00	2	1	9	18	111	202	106	3	0	0	0	0	0	0	0	452	31-40	313
11:00	1	0	10	9	83	222	110	11	0	0	0	0	0	0	0	446	36-45	332
12 PM	4	3	14	25	101	226	113	16	2	0	0	0	0	0	0	504	36-45	339
13:00	1	0	8	15	105	235	106	10	1	0	0	0	0	0	0	481	36-45	341
14:00	24	1	31	105	147	170	52	7	0	0	0	0	0	0	0	537	31-40	318
15:00	2	4	35	66	224	263	63	5	0	0	0	0	0	0	0	662	31-40	487
16:00	7	2	35	114	321	249	63	4	0	0	0	0	0	0	0	795	31-40	570
17:00	29	38	78	221	380	166	32	1	0	0	0	0	0	0	0	945	26-35	602
18:00	190	100	92	178	172	56	4	1	0	0	0	0	0	0	0	793	26-35	355
19:00	0	2	19	134	301	142	24	4	1	0	0	0	0	0	0	627	31-40	443
20:00	3	1	7	39	134	113	27	2	0	0	0	0	0	0	0	326	31-40	247
21:00	1	0	14	27	114	111	30	3	0	0	0	0	0	0	0	300	31-40	225
22:00	0	0	9	16	76	71	23	3	0	0	0	0	0	0	0	198	31-40	147
23:00	0	0	4	5	22	36	29	3	1	1	0	0	0	0	0	101	36-45	65
<b>Total</b>	<b>318</b>	<b>211</b>	<b>539</b>	<b>1252</b>	<b>2991</b>	<b>3327</b>	<b>1296</b>	<b>120</b>	<b>8</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10064</b>		
Percent	3.2%	2.1%	5.4%	12.4%	29.7%	33.1%	12.9%	1.2%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	08:00	08:00	07:00	07:00	08:00	07:00	09:00	09:00	01:00	07:00								07:00
Vol.	46	40	70	128	250	283	153	20	1	1								852
PM Peak	18:00	18:00	18:00	17:00	17:00	15:00	12:00	12:00	12:00	23:00								17:00
Vol.	190	100	92	221	380	263	113	16	2	1								945
<b>Total</b>	<b>318</b>	<b>211</b>	<b>539</b>	<b>1252</b>	<b>2991</b>	<b>3327</b>	<b>1296</b>	<b>120</b>	<b>8</b>	<b>2</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>0</b>	<b>10064</b>		
Percent	3.2%	2.1%	5.4%	12.4%	29.7%	33.1%	12.9%	1.2%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			

15th Percentile : 26 MPH  
 50th Percentile : 34 MPH  
 85th Percentile : 39 MPH  
 95th Percentile : 43 MPH

Stats  
 10 MPH Pace Speed : 31-40 MPH  
 Number in Pace : 6331  
 Percent in Pace : 62.9%  
 Number of Vehicles > 40 MPH : 1426  
 Percent of Vehicles > 40 MPH : 14.2%  
 Mean Speed(Average) : 34 MPH

# Greater Traffic Company

Winters Chapel Rd just Northwest  
of Dekalb County Water plant gate.

nb, sb	Start Time	0	15	16	20	21	25	26	30	31	35	36	40	41	45	46	50	51	55	56	60	61	65	66	70	71	75	76	9999	Total	Pace Speed	Number in Pace
03/09/16		2	0	2	1	17	29	29	11	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	94	36-45	58	
01:00		0	0	2	1	9	17	18	8	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	58	36-45	35	
02:00		0	0	0	1	6	16	15	4	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	42	36-45	31	
03:00		0	0	1	0	6	10	8	9	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	35	35-44	18	
04:00		0	1	0	2	9	19	17	7	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	55	36-45	36	
05:00		1	1	7	7	13	44	46	26	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	150	36-45	90	
06:00		5	7	46	56	93	194	208	69	19	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	697	36-45	402	
07:00		17	23	<b>94</b>	<b>180</b>	313	411	271	112	29	<b>1</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1451	31-40	725	
08:00		<b>50</b>	<b>41</b>	56	113	<b>389</b>	<b>464</b>	320	126	16	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	<b>1575</b>	31-40	855	
09:00		0	4	11	26	132	392	<b>439</b>	<b>150</b>	27	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1181	36-45	831	
10:00		2	2	12	21	125	235	246	126	23	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	792	36-45	481	
11:00		1	0	10	13	101	259	259	<b>127</b>	<b>39</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	809	36-45	518	
12 PM		8	9	21	44	125	265	250	<b>147</b>	<b>45</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	914	36-45	515	
13:00		2	1	12	30	155	325	281	106	16	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	928	36-45	606	
14:00		31	6	39	137	190	266	239	95	20	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1023	36-45	506	
15:00		7	5	40	81	275	430	<b>308</b>	94	16	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1256	36-45	738	
16:00		24	16	62	145	433	432	258	94	17	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1481	31-40	866	
17:00		35	50	98	<b>268</b>	<b>545</b>	<b>454</b>	304	75	10	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	<b>1839</b>	31-40	1000	
18:00		<b>195</b>	<b>110</b>	<b>119</b>	255	376	325	153	42	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1583	31-40	706	
19:00		4	7	47	196	398	320	159	30	6	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1167	31-40	718	
20:00		3	1	8	55	210	246	171	27	8	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	729	31-40	456	
21:00		1	1	17	38	152	238	143	44	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	639	31-40	390	
22:00		0	1	12	30	95	110	109	33	9	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	399	36-45	219	
23:00		0	0	4	5	35	58	73	35	5	<b>2</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	217	36-45	131	
Total		388	286	720	1705	4202	5559	4324	1597	330	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19114			
Percent		2.0%	1.5%	3.8%	8.9%	22.0%	29.1%	22.6%	8.4%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				
AM Peak	08:00	08:00	07:00	07:00	08:00	08:00	08:00	09:00	09:00	11:00	07:00																			08:00		
Vol.		50	41	94	180	389	464	439	150	39	1																			1575		
PM Peak	18:00	18:00	18:00	17:00	17:00	17:00	17:00	15:00	12:00	12:00	23:00																			17:00		
Vol.		195	110	119	268	545	454	308	147	45	2																			1839		
Total		388	286	720	1705	4202	5559	4324	1597	330	3	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	19114			
Percent		2.0%	1.5%	3.8%	8.9%	22.0%	29.1%	22.6%	8.4%	1.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%				

15th Percentile : 29 MPH  
 50th Percentile : 37 MPH  
 85th Percentile : 43 MPH  
 95th Percentile : 48 MPH

Stats  
 10 MPH Pace Speed : 36-45 MPH  
 Number in Pace : 9896  
 Percent in Pace : 51.8%  
 Number of Vehicles > 40 MPH : 6254  
 Percent of Vehicles > 40 MPH : 32.7%  
 Mean Speed(Average) : 37 MPH



**PEDESTRIAN CROSSING STUDY  
E. JONES BRIDGE ROAD AT  
AVALA PARK LANE/BROADGREEN DRIVE**

CITY OF PEACHTREE CORNERS  
GWINNETT COUNTY, GEORGIA

W & A Project No. 16-TF-006  
CIP PROJECT #PTC 15.17

April 8<sup>th</sup>, 2016

Prepared By:

Wolverton & Associates, Inc.  
6745 Sugarloaf Parkway, Suite 100  
Duluth, GA 30097  
770-447-8999 (Tel)  
770-447-9070 (Fax)

Mr. Todd DeVos, P.E., PTOE  
Todd.devos@Wolverton-assoc.com

Prepared For:

Mr. Greg Ramsey, P.E.  
Director of Public Works  
City of Peachtree Corners  
147 Technology Parkway, Suite 200  
Peachtree Corners, GA 30092

WOLVERTON & ASSOCIATES, INC.  
6745 SUGARLOAF PARKWAY  
SUITE 100  
DULUTH, GA 30097  
(770) 447-8999 PHONE  
(770) 447-9070 FAX  
[www.wolverton-assoc.com](http://www.wolverton-assoc.com)

# TABLE OF CONTENTS

---

TABLE OF CONTENTS .....	i
LIST OF FIGURES.....	ii
LIST OF TABLES .....	ii
<b>1. INTRODUCTION .....</b>	<b>1</b>
Methodology.....	1
<b>2. EXISTING CONDITIONS.....</b>	<b>2</b>
Study Area Characteristics.....	2
Existing Traffic Flow Data.....	3
<b>3. POTENTIAL SOLUTIONS/ANALYSIS .....</b>	<b>6</b>
<b>4. CONCLUSIONS &amp; RECOMMENDATIONS.....</b>	<b>13</b>
<b>5. REFERENCES .....</b>	<b>15</b>
<b>6. APPENDICES .....</b>	<b>16</b>

# LIST OF FIGURES

---

Figure 1 – Study Area Map .....	1
Figure 2 – Study Area Existing Travel Lanes and Traffic Control .....	2
Figure 3 – Existing Traffic Flow Data & Observed Pedestrian Crossing Locations.....	5
Figure 4 – Example of Pedestrian Crossing with No Flashers .....	6
Figure 5 – Example of Pedestrian Crossing with .....	7
Figure 6 – Example of Pedestrian Crossing with Wig Wag Flashers Active by Push Button .....	8
Figure 7 – Example of Pedestrian Crossing with Pedestrian Activated Rapid Flashing Beacon .....	9
Figure 8 – Example of Pedestrian Crossing with Hawk Signal .....	10
Figure 9 – Guidelines for Installing Hawk Signal (High Speed Roadways).....	11
Figure 10 – Warrant 4 (70% Factor), Traffic Signal Warrants Analysis.....	12
Figure 11 – Proposed Location: Marked Crosswalk With RFB.....	14

# LIST OF TABLES

---

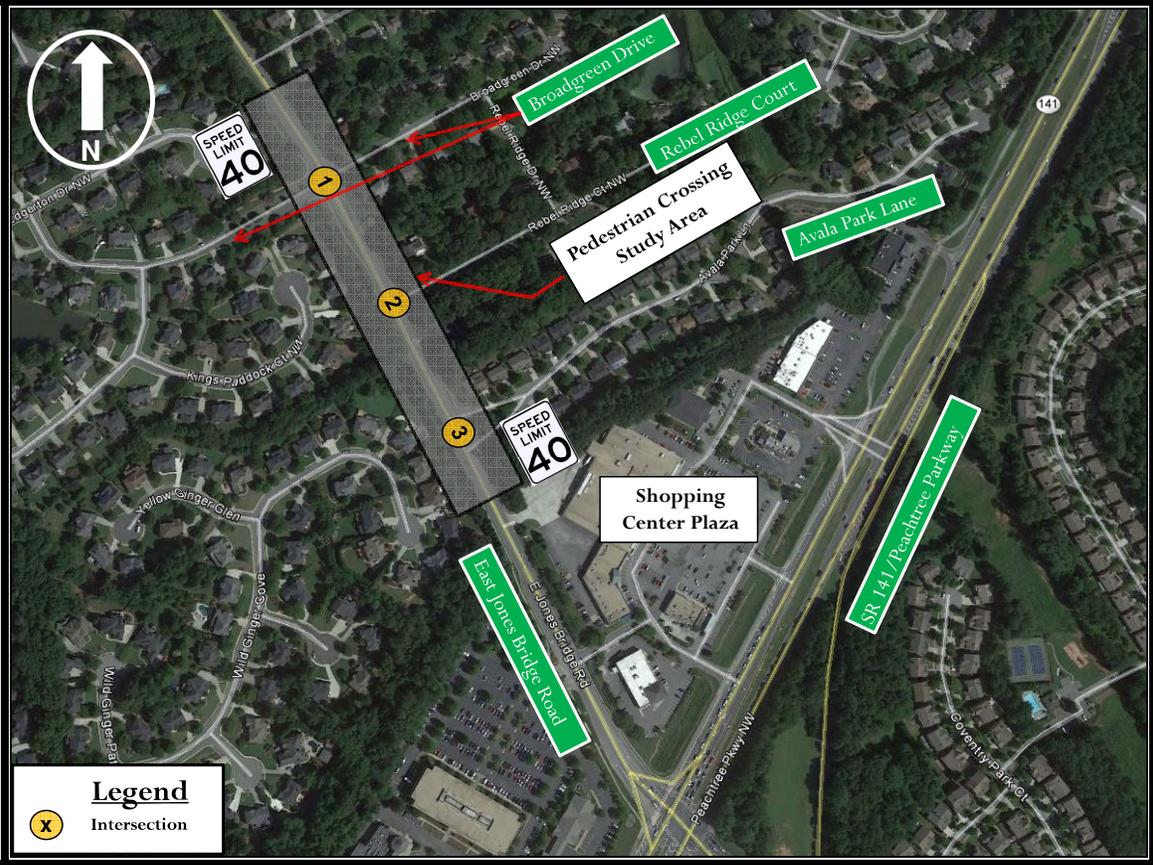
Table 1 – Pedestrian Data Collection Results .....	4
--	---

# 1. INTRODUCTION

## PEDESTRIAN CROSSING STUDY – EAST JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

This pedestrian crossing study was conducted to evaluate the pedestrian activity crossing East Jones Bridge Road in the vicinity of Avala Park Lane and Broadgreen Drive. This study analyzes the various options that can be utilized to provide a safer method of crossing East Jones Bridge Road as well as the best location for the crossing within the study areas project limits. The study area is shown in Figure 1.

Figure 1 – Study Area Map



### Methodology

Initial evaluations were made of the various types of pedestrian crossings utilized in Georgia. The Georgia Department of Transportation (GDOT) and the Manual on Uniform Traffic Control Devices (MUTCD) specify criteria for deciding where, if, and the type of marked crosswalk that should be installed. The applicable criteria required to warrant approved types of pedestrian crossings was gathered and was used in identifying the necessary data. The collected data included Pedestrian count data, vehicular count data, and vehicular speed data. All data were collected on typical weekdays with good weather to provide accurate count data for the analysis. The study area was then analyzed to determine if a need exists for installing a crosswalk treatment and then if so, the best location and type of crossing for the area.

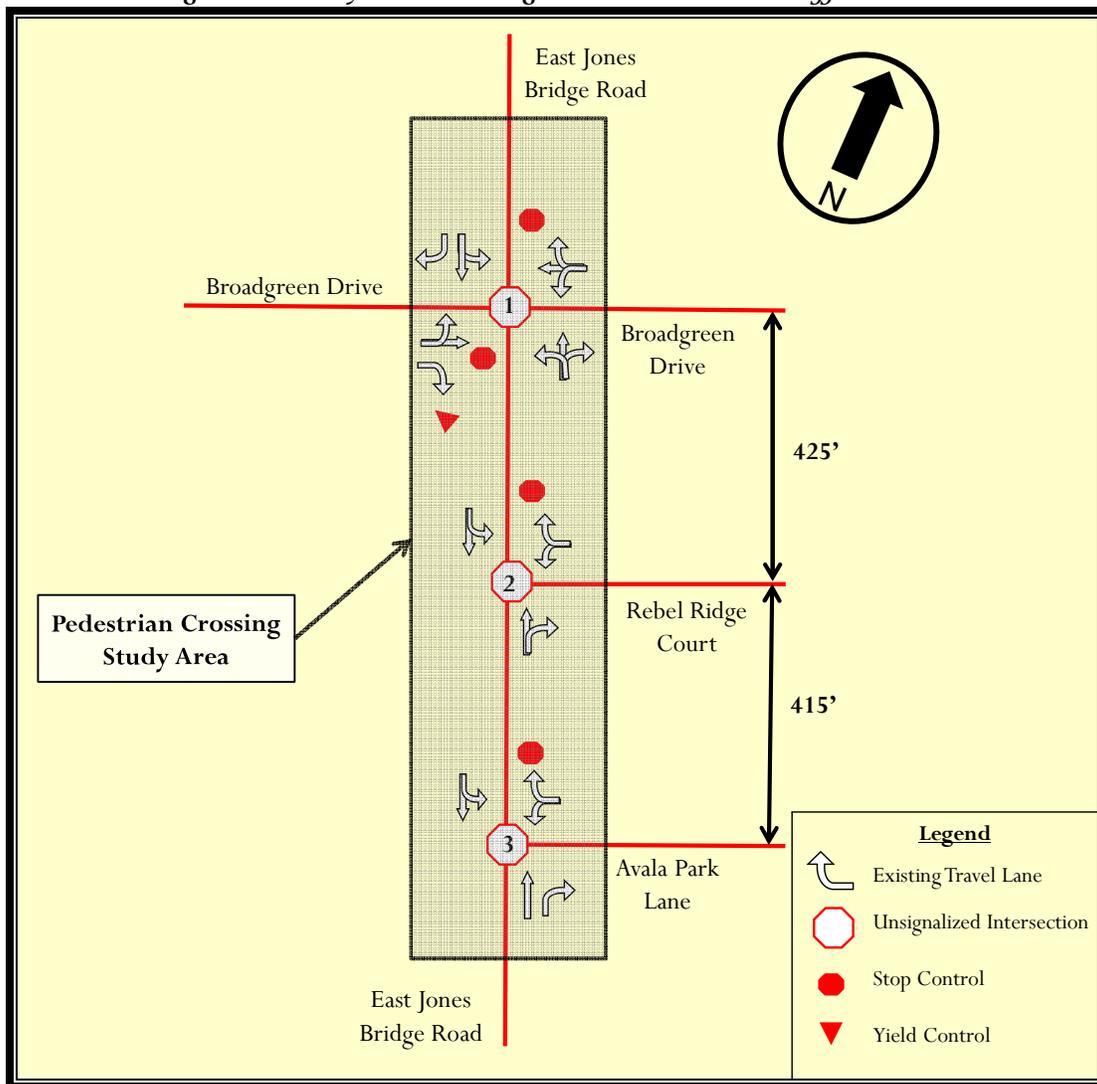
## 2. EXISTING CONDITIONS

### PEDESTRIAN CROSSING STUDY – EAST JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

#### Study Area Characteristics

The study area along East Jones Bridge Road is located between Broadgreen Drive to the north and Avala Park Lane to the south. This encompasses a quarter of a mile section of East Jones Bridge Road located in the northwest portion of the City of Peachtree Corners along the western edge of SR 141/Peachtree Parkway. East Jones Bridge Road is a two lane roadway in the vicinity of the study area. The speed limit on East Jones Bridge Road is posted at 40 mph. The existing lane geometry and traffic control for the three intersections located inside the study area are shown below in Figure 2.

Figure 2 – Study Area Existing Travel Lanes and Traffic Control



Broadgreen Drive (Intersection #1) forms a four-legged intersection with East Jones Bridge Road. The eastbound approach serves as one of the two access points to a series of residential cul-de-sacs', and the westbound approach serves a residential cul-de-sac. Both the eastbound and westbound approaches of Broadgreen Drive are stop sign controlled and East Jones Bridge Road is free flow.

Rebel Ridge Court (Intersection #2) is a residential culdesac that forms a T-shaped intersection with East Jones Bridge Road, and is located to the immediate south of Broadgreen Drive. Rebel Ridge Court is stop sign controlled and East Jones Bridge Road is free flow. Finally, Avala Park Lane (Intersection #3) serves a residential cul-de-sac that forms a T-shaped intersection with East Jones Bridge Road, and is located to the immediate south of Rebel Ridge Court. Avala Park Lane is stop signed controlled and East Jones Bridge Road is free flow. The intersection of Rebel Ridge Court with East Jones Bridge Road is located approximately 425 feet south of the Broadgreen Drive intersection, and 415 feet north of the Avala Park Lane intersection (measured Centerline to Centerline).

There are sidewalks located along both the east and west sides of East Jones Bridge Road. There are currently no marked crosswalks within the study area. The nearest crosswalk to cross East Jones Bridge Road is located approximately 1,125 feet to the south of Avala Park Lane at the signalized intersection of SR 141/Peachtree Parkway and East Jones Bridge Road/Medlock Bridge Road. Although this signal provides pedestrian access across East Jones Bridge Road, the intersection is considerably out of the pedestrian pathway and located over a 1,000 feet south of the study area. Therefore, the signal is not considered a feasible pedestrian crossing location for pedestrians within the study area. If a pedestrian facility in the study area is warranted, it would funnel the pedestrians in the area to one common point and cross pedestrians on East Jones Bridge Road in a safe manner.

### ***Existing Traffic Flow Data***

24-hour Vehicle tube count and vehicle speed data were collected on Wednesday, March 9<sup>th</sup>, 2016 inside the study area. Additionally, the number of pedestrians that entered the study area was counted, as well as the number of pedestrians who crossed East Jones Bridge Road within the study area. Pedestrian counts were taken during the AM Peak period (6:30 AM to 8:30 AM) and during the PM Peak period (4:00 PM to 6:00 PM).

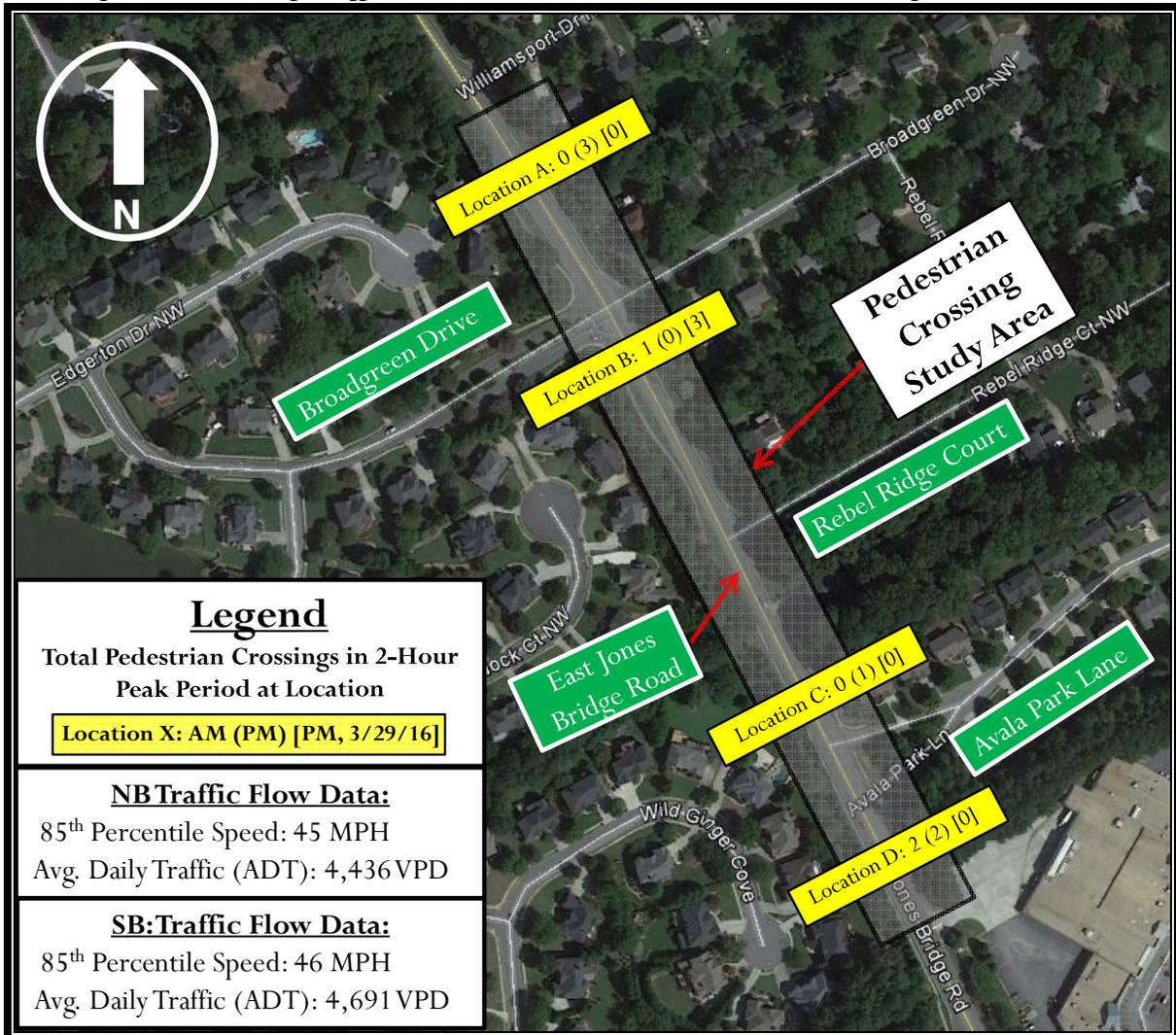
Pedestrian counts were collected on Tuesday, February 16<sup>th</sup>, 2016 and March 29<sup>th</sup>, 2016 for the PM Peak period, and on Wednesday, February 17<sup>th</sup>, 2016 for the AM Peak period. The PM Peak period pedestrian counts were conducted twice due to daylight savings time. Pedestrian activity during the March, 29<sup>th</sup> counts was similar to the February, 16<sup>th</sup> counts. Table 1 shows the number of pedestrians that crossed East Jones Bridge Road as well as the number of pedestrians that did not cross East Jones Bridge Road. The pedestrians that did not cross East Jones Bridge Road did not influence the crossing type of location, but are shown for informational purposes to show that additional pedestrian activity is present along the corridor. Weather conditions during the AM peak period data collection on February, 16<sup>th</sup> was sunny & 40 degrees Fahrenheit. Weather conditions during the PM peak period data collection on February, 17<sup>th</sup> and March 29<sup>th</sup> was sunny & 60 degrees Fahrenheit and sunny & 75 degrees Fahrenheit.

*Table 1 – Pedestrian Data Collection Results*

<b>Location #1: East Jones Bridge Road Between Broadgreen Drive &amp; Avala Park Lane</b>		
Time Period	PEDs Parallel to East Jones Bridge Rd (Not Crossing)	PEDs Crossing East Jones Bridge Rd
<b>AM PEAK: Observed 2/16/16</b>		
6:30 AM - 7:00 AM	5	0
7:00 AM - 7:30 AM	1	1
7:30 AM - 8:00 AM	3	2
8:00 AM - 8:30 AM	6	0
Total	15	3
<b>PM PEAK: Observed 2/17/16</b>		
4:00 PM - 4:30 PM	4	1
4:30 PM - 5:00 PM	7	2
5:00 PM - 5:30 PM	8	2
5:30 PM - 6:00 PM	6	1
Total	25	6
<b>PM PEAK: Observed 3/29/16</b>		
4:00 PM - 4:30 PM	9	2
4:30 PM - 5:00 PM	1	0
5:00 PM - 5:30 PM	5	1
5:30 PM - 6:00 PM	2	0
Total	17	3

The pedestrians that did cross were observed to cross East Jones Bridge Road in specific locations. Figure 3 shows a breakdown of where along the corridor the pedestrian crossing activity occurred. There were various locations where pedestrian crossings occurred within in the study area. The crossings were not concentrated at one location. This is due to the fact that there is no designated area for pedestrians to currently cross. Figure 3 shows the four (4) observed crossing locations that occurred during the initial AM and PM pedestrian counts in Mid-February and late March, 85<sup>th</sup> percentile speed and average daily traffic per direction.

*Figure 3 – Existing Traffic Flow Data & Observed Pedestrian Crossing Locations*



### 3. POTENTIAL SOLUTIONS/ANALYSIS

#### PEDESTRIAN CROSSING STUDY – EAST. JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

There are various types of pedestrian crossings that can be utilized to allow for a safer pedestrian crossing than what currently exists in the study area today. The following is a list of the various types of crossing and descriptions of their operation. While GDOT’s approval of a pedestrian crossing treatment installation, if applicable, is not required since East Jones Bridge Road is not a designated state route, this report still uses GDOT’s criteria for installing a pedestrian crossing treatment. These various types of pedestrian crossings vary in amount of warning for the vehicle, the indications for the vehicle and pedestrian, and the method of activation.

##### *Treatment #1: Pedestrian Sign with No Flashing Beacons*

The pedestrian sign with no flashing beacons is comprised of a crosswalk with a static pedestrian sign (W11-2). This type of crossing advises the driver of potential pedestrian activity in the vicinity: It provides identification of a crosswalk, but does not give drivers a different message when there is a pedestrian present at the crosswalk. This type of crossing is utilized in lower pedestrian volume areas and sections of roadway that have no sight distance restrictions, whereby providing drivers with a clear line of sight to the crosswalk. It should be noted that there are no required pedestrian volumes or vehicular speeds and volumes to warrant this type of crossing treatment. However, this type of treatment is typically used in low pedestrian volume locations. The highest maximum pedestrian crossing volume at any hour of the day between the two days of pedestrian data collection is four (4). Therefore, this type of pedestrian treatment can be utilized. Figure 4 shows the intersection of Peeler Road and Lakeside Drive in Dunwoody, which is an example of a pedestrian crossing with no flashing beacons.

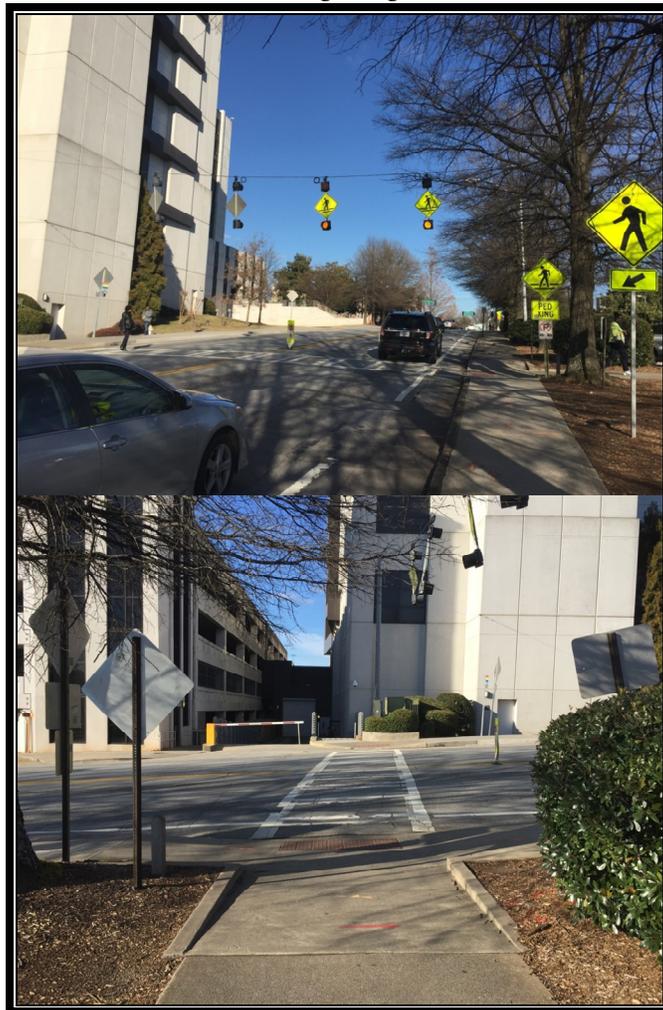
*Figure 4 – Example of Pedestrian Crossing with No Flashers*



***Treatment #2: Pedestrian Sign with Wig Wag Flashers***

The pedestrian sign with wig wag flasher type crossing is comprised of a crosswalk with a pedestrian sign (W11-2) that includes wig wag flashers on both vehicular approaches. The wig wag flashers operate continuously (24-hours a day) and thus are not activated by the presence of a pedestrian. No pedestrian button is present at this type of crossing. This type of crossing warns the driver of a potential pedestrian crossing but does not operate any differently with a pedestrian present or absent. This type of crossing is installed in lower pedestrian volume areas and provides a warning to the driver that pedestrian activity is common in the area. However, there are no specific pedestrian volumes or vehicular volume and speed criteria to warrant this type of crossing treatment. The highest maximum pedestrian crossing volume at any hour of the day between the two days of pedestrian data collection is four (4). Therefore, this type of pedestrian treatment can be utilized. Figure 5 shows an example of a midblock pedestrian crossing in downtown Decatur, which has continuous wig wag flashers.

***Figure 5 – Example of Pedestrian Crossing with Continuous Wig Wag Flashers***



***Treatment #3: Pedestrian Sign with Wig Wag Flashers Activated by Pedestrian***

The pedestrian sign with wig wag flasher type crossing is comprised of a crosswalk with a pedestrian sign (W11-2) that includes wig wag flashers on both vehicular approaches. The wig wag flashers operate for a short duration of time only when there is the presence of a pedestrian. The flashers are activated by a pedestrian push button, which is present at this type of crossing. This type of crossing warns the driver of a pedestrian crossing at this location. This type of crossing is utilized in lower pedestrian volume areas and provides a warning to the driver that pedestrian activity is common in the area. The pushbutton activation feature warns a driver that a pedestrian is either waiting to cross, in the process of crossing, or has just finished crossing. This type of installation is no longer utilized, and has been replaced with Treatment #4. Figure 6 shows the midblock pedestrian crossing on Peachtree Corners Circle located just northeast of the intersection of with Eastman Trail in Peachtree Corners. Treatment #4, which consists of a Pedestrian sign with Rapid Flashing Beacon (RFB), is described next.

***Figure 6 – Example of Pedestrian Crossing with Wig Wag Flashers Active by Push Button***



***Treatment #4: Pedestrian Sign with Rapid Flashing Beacon (RFB)***

The pedestrian sign with RFB is comprised of a crosswalk with a pedestrian sign (W11-2) that includes an RFB. The beacon is activated by a pedestrian button located at each end of the crosswalk. The RFB remains dark until activated by the button. A center refuge island can be located in the center of the roadway to shorten the crossing distance and provide a safer crossing. No indications are present for the pedestrian. This type of crossing can be used in lower to moderate pedestrian volume areas and provides a warning to the driver when a pedestrian is present. However, there are no specific pedestrian volumes or vehicular volume and speed criteria to warrant this type of crossing treatment. Figure 7 shows the midblock crossing with an RFB on Tilly Mill Road at the city limit of the city of Dunwoody.

***Figure 7 – Example of Pedestrian Crossing with Pedestrian Activated Rapid Flashing Beacon***



*Treatment #5: Hawk Signal*

A Hawk signal is a High intensity Activated cross Walk. Depending upon the width of the roadway and the median size, the crossing can be a one or two stage crossing. A pedestrian utilizing the crossing presses the button to activate the first signal, when the traffic signal turns red, a “walk” indication allows the pedestrian to cross the first stage. Once reaching the center refuge island the pedestrian presses the second button to activate the second signal. The vehicular indication is a three section signal head with two side by side red indications above a yellow indication. The vehicular signals are dark until activated by the pedestrian, while the pedestrian signals are a solid orange hand (don’t walk) indication. Once activated by the pedestrian, the vehicular signal flashes yellow to warn drivers that the signal has been activated. The vehicular signal then turns to solid yellow to prepare the drivers to stop. Both of the red vehicular indications turn solid red while the pedestrian signal changes to a white walking man. After the walk time has elapsed the pedestrian clearance times out, the pedestrian signal changes to a flashing orange hand and a countdown timer, while the vehicular indication changes to a flashing red. The flashing red allows a vehicle to proceed through the crosswalk after the pedestrian has crossed its path. After the countdown time has elapsed, the signals revert back to solid orange hand for the pedestrian and dark for the vehicle. Figure 8 shows the midblock signalized crossing along SR 155/Candler Road in Decatur, which is an example of a pedestrian crossing with a hawk signal.

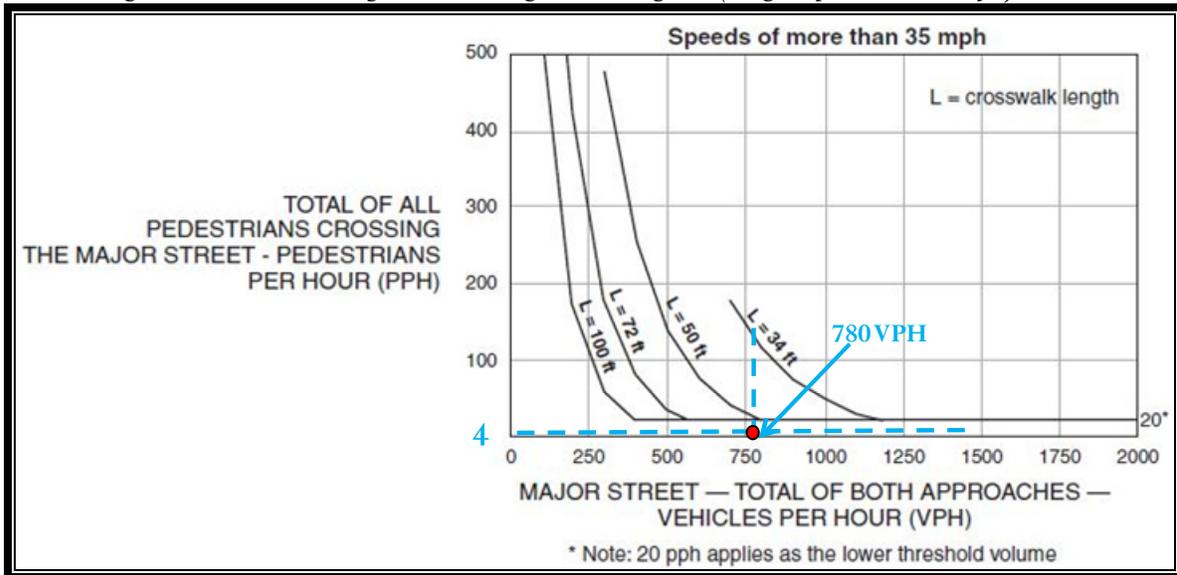
*Figure 8 – Example of Pedestrian Crossing with Hawk Signal*



Guidelines for the installation of pedestrian signals on high-speed roadways (speeds greater than 35 MPH) were applied from the MUTCD for the observed crossing locations within the study area, and are shown below. The posted speed limit along this section of Winters Chapel Road is 40 MPH and the overall 85<sup>th</sup> Percentile speed was measured to be 46 MPH. Therefore, Figure 9 is utilized for determining if a Hawk signal is warranted at a particular location. Each curve has an associated expected crosswalk length (L), if a marked crosswalk were to be installed. East Jones Bridge Road is a two lane two way roadway. The approximate curb to curb crossing distance at any of the crossing locations within the study area is approximately 25 feet. The highest one hour peak pedestrian crossings occurred between 4:30 PM and 5:30 PM, and totaled four (4) crossings. The bi-directional traffic volume between 4:30 PM and 5:30 PM is 780 vehicles per hour.

As can be seen, the combination of the major street total bi-directional traffic and the number of pedestrians crossing in the peak hour falls below the applicable curve. Since the number of pedestrian crossings during the peak hour falls below the minimum threshold of 20 pedestrian crossings per hour, the warrant for installing a Hawk Signal is NOT met for installation.

*Figure 9 – Guidelines for Installing Hawk Signal (High Speed Roadways)*



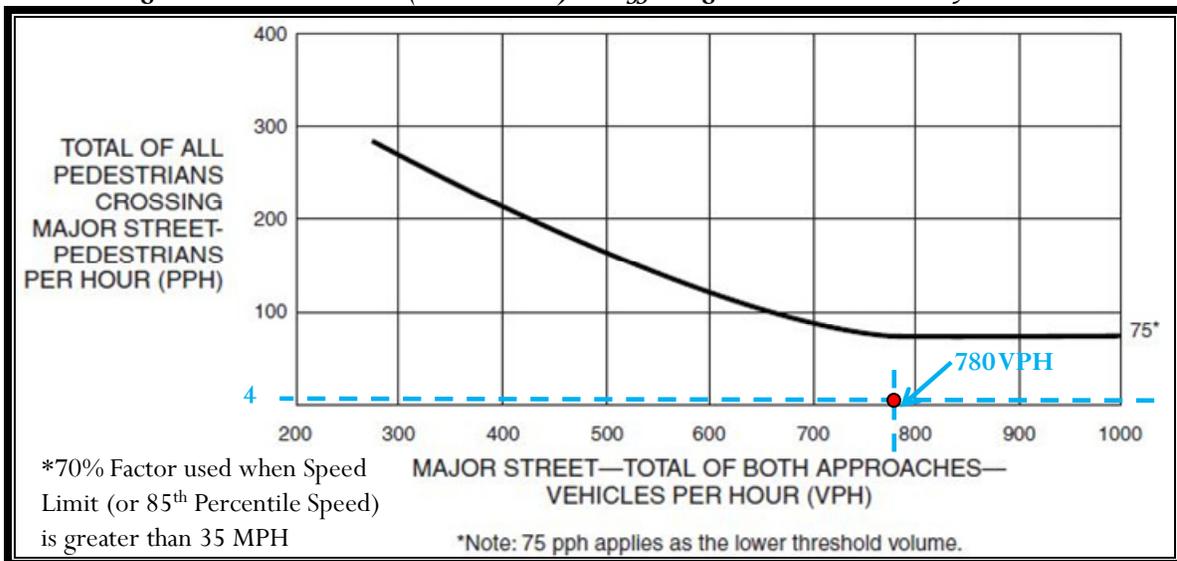
### Treatment #6: Traffic Signal

The traffic signal provides the highest level of pedestrian crossing protection across all approaches to an intersection that are signalized. Both vehicles and pedestrians each have their own signal indication. A pedestrian utilizing a crossing at a signalized intersection presses the push button to place a call into the signal controller. Depending upon the programming of the signal at the intersection, the pedestrian is given a “walk” indication either at the same time as the adjacent vehicular movement (e.g. concurrent pedestrian phase) or by itself (e.g. exclusive pedestrian phase). After the “walk” indication is given, a flashing hand with or without a countdown of time remaining to cross before right of way is given to another direction and vehicular movement.

Warrant four (4) in the traffic signal warrants section of the MUTCD is shown below. It should be noted that the 70% pedestrian volume (e.g. minimum of 75 pedestrian crossings per hour) can be used since the measured 85<sup>th</sup> percentile speed is greater than 35 MPH. Figure 10 shows the results of the pedestrian crossing warrant. The highest one hour peak pedestrian crossings occurred between 4:30 PM and 5:30 PM, and totaled four (4) crossings. The bi-directional traffic volume between 4:30 PM and 5:30 PM is 780 vehicles per hour.

As can be seen, the combination of the major street total bi-directional traffic and the number of pedestrians crossing in the peak hour falls below the curve. Since the number of pedestrian crossings during the peak hour falls below the minimum threshold of 75 pedestrian crossings per hour, Warrant 4 is NOT met for the installation of a standard traffic signal.

Figure 10 – Warrant 4 (70% Factor), Traffic Signal Warrants Analysis



## 4. CONCLUSIONS & RECOMMENDATIONS

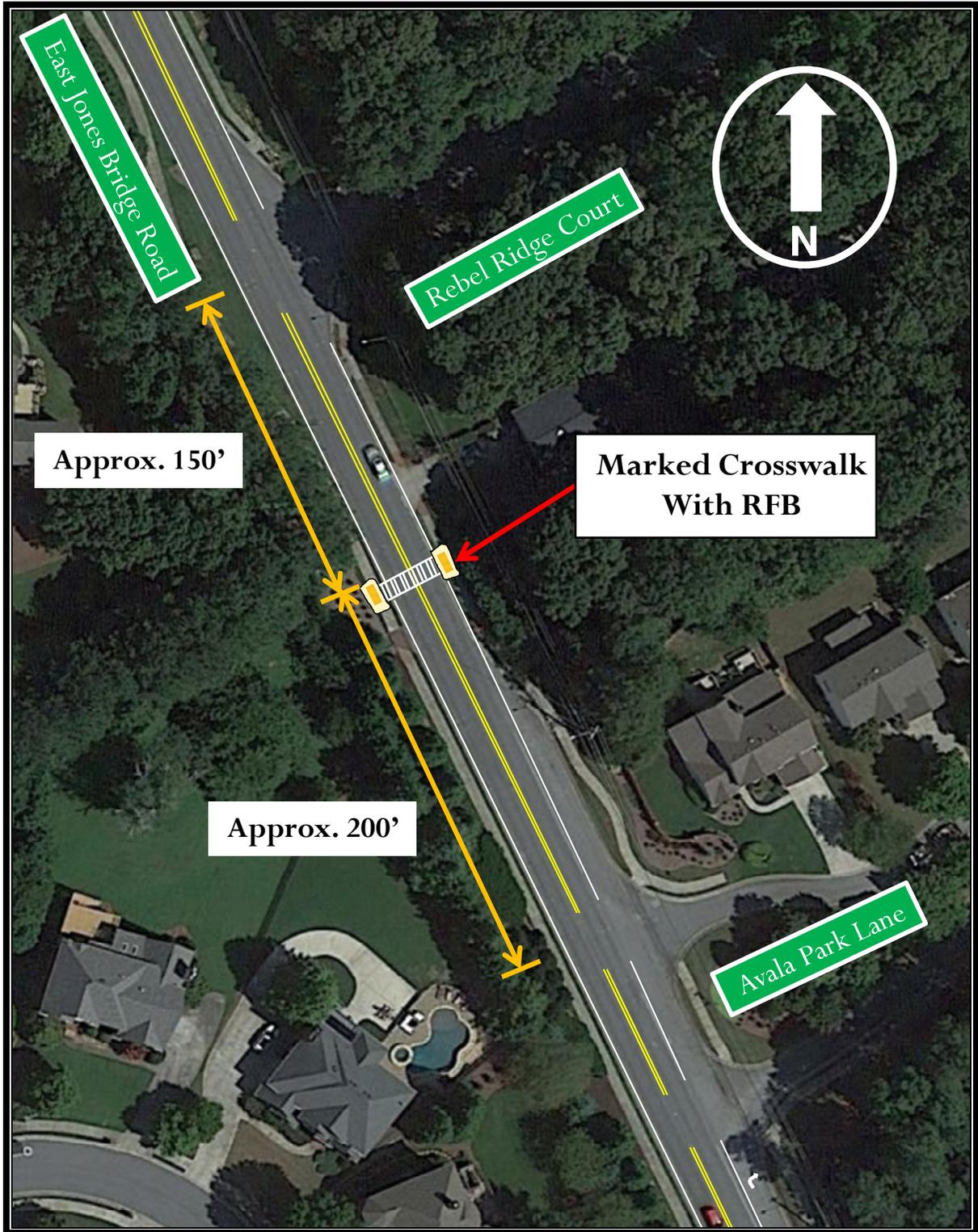
---

### PEDESTRIAN CROSSING STUDY – EAST. JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

An analysis of crash data revealed no pedestrian related accidents or fatalities within the study area. Stopping sight distance appears adequate at all four (4) of the observed crossing locations within the study area. Based upon the analyses of peak hour data, both a traffic signal (Treatment #6) and a Hawk Signal (Treatment #5) are not warranted. Since Treatment #3 is no longer installed, the available remaining options are to install Treatments #1, #2, and #4. A standalone crosswalk without any beacons (Treatment #1) could be installed given the observed low pedestrian crossing volume within the study area. Installing Treatment #2 may not be effective given the combination of low typical pedestrian crossing activity and the continuous wig-wag flashers, whereby desensitizing drivers to the presence of a pedestrian. An RFB (Treatment #4) could be an effective installation, due to the pedestrian actuated nature of the device; however, this treatment is not recommended given the low pedestrian crossing volume that occurs during the highest pedestrian crossing hour.

It is recommended that a marked crosswalk with RFB (Treatment #4) be installed within the study area. The majority of the pedestrians were observed to cross in the center to northern portion of the study area. The MUTCD recommends the placement of any type of midblock crosswalk to be at least 100 feet away from driveways that are signal, stop, or yield controlled. The recommended location of the proposed marked crosswalk is between Rebel Ridge Court and Avala Park Lane, which provides enough distance between both intersections to satisfy the 100 feet specification. Figure 11 shows a concept drawing of the approximate proposed marked crosswalk location.

Figure 11 – Proposed Location: Marked Crosswalk With RFB



## 5. REFERENCES

---

### PEDESTRIAN CROSSING STUDY – EAST. JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

1. AASHTO A Policy on Geometric Design of Highways and Streets, 6<sup>th</sup> Edition, American Association of State Highway and Transportation Officials, Washington, DC, 2011.
2. Manual on Uniform Traffic Control Devices, 2009 Edition, Federal Highway Administration, Washington, DC, 2009.
3. Traffic Calming: State of the Practice, Chapter 3 – Toolbox of Traffic Calming Measures. Institute of Transportation Engineers (ITE), August 1999.
4. “Safety Effects of Marked Versus Unmarked Crosswalks at Uncontrolled Locations: Final Report and Recommended Guidelines.” Research, Development, and Technology Turner-Fairbank Highway Research Center. U.S. Department of Transportation Federal Highway Administration. McLean, VA, 2005.

## 6. APPENDICES

---

### PEDESTRIAN CROSSING STUDY – EAST. JONES BRIDGE ROAD AT AVALA PARK LANE/BROADGREEN DRIVE

#### APPENDIX A: TRAFFIC COUNTS

**Work Session**  
**State Route 141**  
**G. Ramsey**

## EXECUTIVE SUMMARY

*Wolverton & Associates, Inc.* has developed a concept level study to analyze operational improvements along SR 141/Peachtree Parkway in the City of Peachtree Corners. SR 141/Peachtree Parkway is a heavily congested corridor and experiences volume-to-capacity issues during the peak hours of the day. The four (4) signalized intersections included within the project limits are SR 141/Peachtree Parkway at Spalding Drive, Peachtree Corners Circle, The Forum Driveway and East Jones Bridge/Medlock Bridge Road.

This project analyzes a variety of options including additional lanes of capacity, implementing “Michigan left turn” or “Median U-turns” treatments as an alternative way of re-routing left turn movements, adding and removing signals, implementing Flashing Yellow Arrow (FYA) signal heads, and implementing a Continuous Flow Intersection (CFI). Regardless of which type of operational improvement is analyzed or considered, it is recommended an additional through lane of capacity be constructed along SR 141/Peachtree Parkway. The project report analyzed nine (9) Build Option Scenarios.

**(Build Options 1-3).** Re-directing either mainline, side street or both mainline and side street left turns from Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to median U-turn signals. It is not recommended to implement median U-turn traffic signals along SR 141/Peachtree Parkway unless a 3<sup>rd</sup> through lane is constructed both northbound and southbound.

**(Build Option 4).** Add a mainline through lane of capacity along SR 141/Peachtree Parkway northbound and southbound. It is recommended this improvement be considered to be implemented.

**(Build Option 5).** At Spalding Drive, add a side street through lane of capacity and median U-turns signals along mainline of SR 141/Peachtree Parkway for northbound and southbound left turns. It is recommended this improvement be considered to be implemented.

**(Build Option 6).** Add a traffic signal on the west leg of the intersection at The Forum, construct a median on west leg of intersection to control access, and re-route side street left turns. It is recommended to construct the traffic signal and construct the median for possible safety and access control. Not recommended to re-route side street left turn due to volume-to-capacity concerns.

**(Build Option 7).** Studied removing The Forum full access traffic signal, replace with RiRo access and construct ideally spaced median U-turn signals along SR 141/Peachtree Parkway. It is recommended this operational change not be considered due to lack of future development data for the east side of The Forum intersection.

**(Build Option 8).** Implement a CFI for the mainline southbound left turns and median U-turn signal for the mainline northbound left turns at East Jones Bridge/Medlock Bridge Road. It is recommended this improvement be considered to be implemented.

**(Build Option 9).** Study eligible locations for FYA signal implementation. It is recommended this improvement be considered to be implemented.

The conclusions drawn in this report are done at the concept level. If one or more of the recommendations are considered to be implemented, an additional GDOT Corridor Study will have to be completed given this corridor resides along a State Route and GDOT has minimum requirements to satisfy the completion of a Corridor Study.



**TRAFFIC ENGINEERING REPORT**  
FOR  
**PROPOSED ROADWAY IMPROVEMENTS**

SR 141/Peachtree Parkway  
City of Peachtree Corners  
Gwinnett County, GA

Prepared for:  
City of Peachtree Corners  
Attn: Greg Ramsey, PE

W&A Project No. 15-TF-035

April 12, 2016



WOLVERTON & ASSOCIATES, INC.  
6745 SUGARLOAF PARKWAY  
SUITE 100  
DULUTH, GA 30097  
(770) 447-8999 PHONE  
(770) 447-9070 FAX  
[www.wolverton-assoc.com](http://www.wolverton-assoc.com)

# TABLE OF CONTENTS

---

LIST OF FIGURES .....	ii
LIST OF TABLES.....	ii
<b>1. INTRODUCTION.....</b>	<b>1</b>
Methodology .....	2
Planned Improvements .....	2
<b>2. EXISTING CONDITIONS .....</b>	<b>4</b>
Roadways .....	4
<b>3. TRAFFIC DATA .....</b>	<b>7</b>
Average Daily Traffic Volumes.....	7
<b>4. CAPACITY ANALYSIS .....</b>	<b>10</b>
Intersection Capacity Analysis Results .....	10
Build Options (1 – 3) .....	12
Build Options (1-3) Median U-Turn Comparisons.....	27
Build Options (4 – 9) .....	29
<b>5. CONCLUSIONS.....</b>	<b>42</b>
<b>REFERENCES.....</b>	<b>44</b>

APPENDIX A – Traffic Data

APPENDIX B – Existing and No-Build Scenario Synchro Printouts

APPENDIX C – Build Option (1) Synchro Printouts

APPENDIX D – Build Option (2) Synchro Printouts

APPENDIX E – Build Option (3) Synchro Printouts

APPENDIX F – Build Option (4) Synchro Printouts

APPENDIX G – Build Option (5) Synchro Printouts

APPENDIX H – Build Option (6) Synchro Printouts

APPENDIX I – Build Option (7) Synchro Printouts

APPENDIX J – Build Option (8) Synchro Printouts

## LIST OF FIGURES

---

Figure 1 – Project Location Map .....	3
Figure 2 – Existing Travel Lanes and Traffic Control .....	6
Figure 3 – Existing Peak Hour Volumes .....	7
Figure 4 – Peak Hour Volumes (No Build) 2021 .....	9
Figure 5 –Mainline Left U-Turn Movement .....	14
Figure 6 – Build Option (1): Mainline Left U-Turn Locations.....	15
Figure 7 – Build Option (1): Mainline Left Volumes Re-Directed.....	16
Figure 8 –Side Street Left U-Turn Movement.....	18
Figure 9 – Build Option (2): Side Street Left U-Turn Locations .....	19
Figure 10 – Build Option (2): Side Street Left Volumes Re-Directed.....	20
Figure 11 –Mainline and Side Street Left U-Turn Movement .....	23
Figure 12 – Build Option (3): Mainline and Side Street Left U-Turn Locations.....	24
Figure 13 – Build Option (3): Mainline and Side Street Left Volumes Re-Directed.....	25
Figure 14 – Build Option (5): Spalding Drive Operational Improvements .....	30
Figure 15 – Build Option (6): Peachtree Corners Circle Operational Improvements.....	33
Figure 16 – Build Option (7): The Forum Operational Improvements .....	36
Figure 17 – Build Option (8): East Jones Bridge Road Operational Improvements .....	39
Figure 18 – Build Option (9): FYA Potential Locations.....	41
Figure 19 – Recommended Operational Improvements .....	43

## LIST OF TABLES

---

Table 1 – Growth Rate.....	8
Table 2 – Level of Service Criteria.....	10
Table 3 – LOS and Delay Capacity Analysis: Existing and No Build 2021 .....	11
Table 4 – LOS and Delay Capacity Analysis: Build Option (1) .....	17
Table 5 – Queuing Analysis: Build Option (1).....	17
Table 6 – LOS and Delay Capacity Analysis: Build Option (2) .....	21
Table 7 – Queuing Analysis: Build Option (2).....	21
Table 8 – LOS and Delay Capacity Analysis: Build Option (3) .....	26
Table 9 – Queuing Analysis: Build Option (3).....	26
Table 10 – Southbound Comparison.....	27
Table 11 – Northbound Comparison .....	27
Table 12 – Overall Through Movement Delay Comparison .....	28
Table 13 – LOS and Delay Capacity Analysis: Build Option (4).....	29
Table 14 – Queuing Analysis: Build Option (4) .....	29
Table 15 – LOS and Delay Capacity Analysis: Build Option (5).....	31
Table 16 – Queuing Analysis: Build Option (5) .....	31
Table 17 – LOS and Delay Capacity Analysis: Build Option (6).....	34
Table 18 – Queuing Analysis: Build Option (6) .....	34
Table 19 – LOS and Delay Capacity Analysis: Build Option (7).....	37
Table 20 – LOS and Delay Capacity Analysis: Build Option (8).....	40
Table 21 – Queuing Analysis: Build Option (8) .....	40

# 1. INTRODUCTION

---

## SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

The purpose of this report is to analyze concept improvements for the SR 141/Peachtree Parkway corridor between Spalding Drive and East Jones Bridge/Medlock Bridge Road in the City of Peachtree Corners, Gwinnett County, Georgia. SR 141/Peachtree Parkway is a heavily congested commuter and commercially active corridor. The Georgia Department of Transportation (GDOT) has identified SR 141/Peachtree Parkway as a Regional Corridor of significance, or “RTOP Corridor”, and thus emphasis moving the traffic along SR 141/Peachtree Parkway as best as possible. SR 141/Peachtree Parkway has experienced significant commercial growth in vicinity of this project and has experienced significant residential growth north of the project area. This project analyzes a variety of options including additional lanes of capacity, implementing “Michigan left turn” treatments or “Median U-turns” as an alternative way of re-routing left and right turn movements, adding and removing signals, implementing Flashing Yellow Arrow (FYA) signal heads, and implementing a Continuous Flow Intersection (CFI). The project is identified as follows:

- Analyze SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road for existing, no build and build options in 2021 year milestones.
  - All analysis periods will include AM and PM peak hour traffic analysis.
  - Build Options include nine (9) scenarios:
1. Re-direct **mainline left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
  2. Re-direct **side street left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
  3. Re-direct **both mainline and side street left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
  4. Add **mainline through lane** of capacity along SR 141/Peachtree Parkway from Spalding Drive through East Jones Bridge/Medlock Bridge.
  5. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to Spalding Drive including **additional side street through lane** of capacity and **median U-turns along mainline** of SR 141/Peachtree Parkway.
  6. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to Peachtree Corners Circle including **creating a median and adding a signal on the west leg of the intersection** as well as re-routing the side street left turns.

7. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and **remove The Forum full access traffic signal** and replace with median U-turn signals.
8. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to East Jones Bridge/Medlock Bridge including **implementing a CFI** for the mainline southbound left turns and median U-turns for mainline northbound left turns.
9. Implementing Flashing Yellow Arrow (**FYA**) **signal heads** on eligible movements at signalized intersections.

In all nine (9) Build Options, pedestrian access is not altered. Pedestrian movements or paths are not analyzed or recommended to be re-directed as part of this project. Only the vehicular movements at these intersections are being modeled.

Figure 1 schematically illustrates the existing roadway facility as well as the location of the study intersections along the SR 141/Peachtree Parkway corridor in the City of Peachtree Corners.

### **Methodology**

Initial evaluations were made to assess the current conditions along the corridor. Peak hour turning movement counts (TMCs) were conducted at the signalized intersections within this project. In addition to the TMCs, 24-hour directional Average Daily Traffic (ADT) counts were taken at select locations along the corridor. Traffic projections for the corridor were developed for the Build Year 2021. Existing, No-Build 2021 and Build 2021 models were developed and analyzed for the study intersections along the corridor.

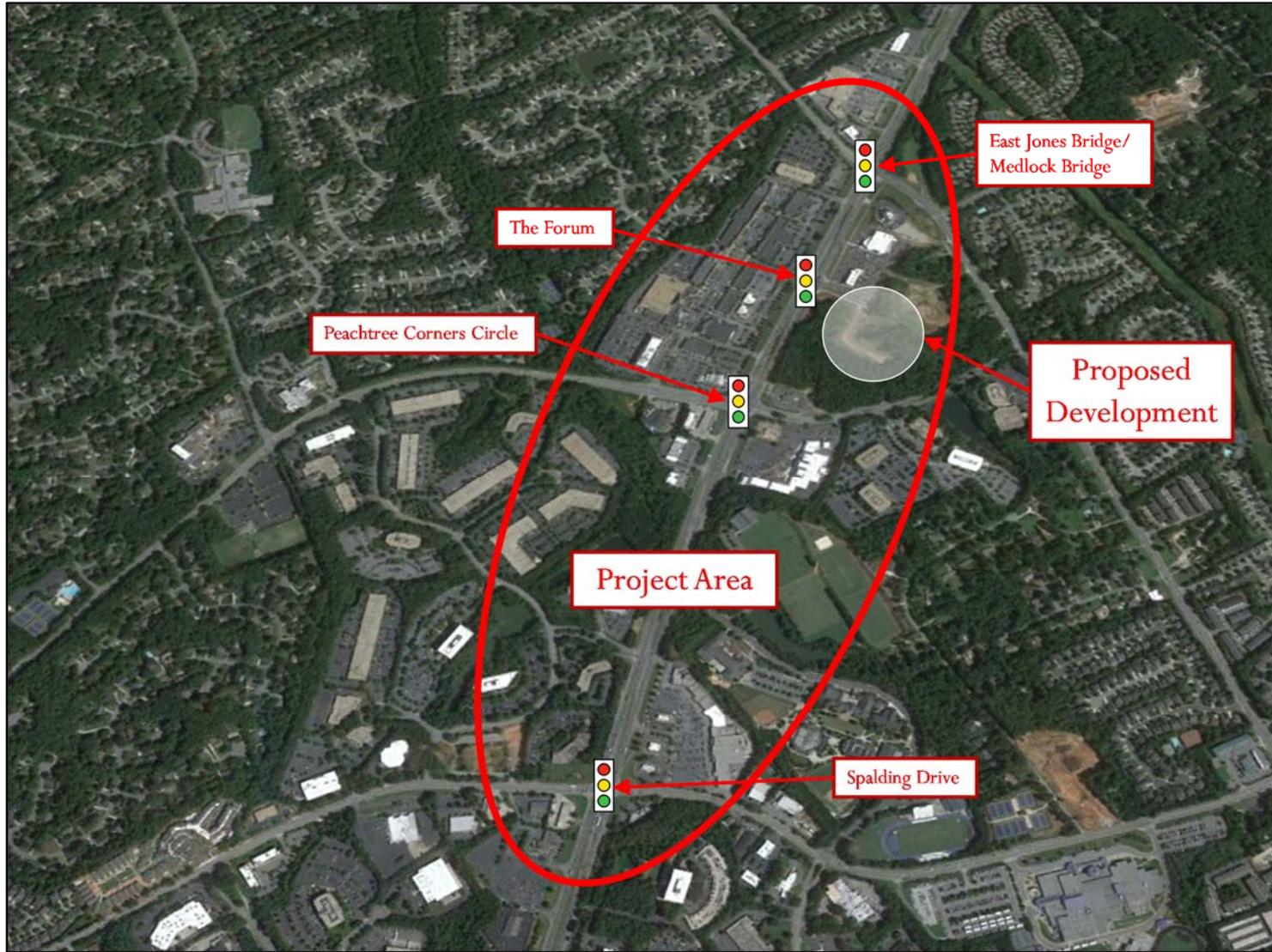
### **Planned Improvements**

In addition to the proposed project, there are other known project that will affect the SR 141/Peachtree Parkway corridor in the City of Peachtree Corners.

The first is a development plan on the east of SR 141/Peachtree Parkway, across from The Forum. This development will be a Town Center that includes retail space, grocery store and apartments. This development is expected to impact SR 141/Peachtree Parkway at The Forum signalized intersection as well as access to Medlock Bridge Road. Some elements of this development have been constructed.

The second project is pedestrian bridge. This bridge will cross over SR 141/Peachtree Parkway between The Forum and Peachtree Corners Circle existing signalized intersections. The bridge is expected to connect The Forum shopping center with the City's new Town Center. The bridge should minimize pedestrian traffic crossing SR 141/Peachtree Parkway at The Forum existing signalized intersection.

Figure 1 – Project Location Map



## 2. EXISTING CONDITIONS

---

### SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

The project will provide analysis for proposed improvements along SR 141/Peachtree Parkway between Spalding Drive and East Jones Bridge/Medlock Bridge Road. Currently, the four (4) signalized intersections studied in the project area are “conventional” eight (8) phase intersections. These intersections have left and through movements that are signalized at each intersection. The project spans 1.4 miles on SR 141/Peachtree Parkway and extend from just south of Spalding Drive to just north of East Jones Bridge/Medlock Bridge Road.

The following are the study intersections along the corridor:

1. SR 141/Peachtree Parkway @ Spalding Drive (signalized)
2. SR 141/Peachtree Parkway @ Peachtree Corners Circle (signalized)
3. SR 141/Peachtree Parkway @ The Forum Driveway (signalized)
4. SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge Road (signalized)

Numerous right-in/right-out (RiRo) driveways as well as median open accesses are present along the corridor with the study area. For reference, the first RiRo begins with #1 south of Spalding Drive and they are numerically labeled going north. Figure 2 illustrates the associated geometry and operation control of all access points in the study area. As general assumptions for all figures in this report, SR 141/Peachtree Parkway is considered to be north/south at all intersections. All other side streets are considered east/west.

#### Roadways

SR 141/Peachtree Parkway is a north/south, median-divided, urban minor arterial with a posted speed limit of 55 mph. This roadway services an area from Peachtree Corners, through Johns Creek to Forsyth County. SR 141/Peachtree Parkway is a connection between I-285 and GA 400. Left and right turn lanes are provided all along this roadway at many of the signalized and unsignalized intersections in the study area. This roadway contains commercial and residential developments along the corridor. A regional shopping center is located on this corridor at The Forum signalized intersection.

Spalding Drive is an east/west urban minor collector with a posted speed limit of 40 mph. This roadway is primarily a two-lane facility with a two way left turn lane (TWLTL) on the east and west side of SR 141/Peachtree Parkway. At the signalized intersection with SR 141/Peachtree Parkway it opens up to one through lane in each direction with dual left turn lanes on both sides of SR 141/Peachtree Parkway. Norcross High School is on Spalding Drive just east of the intersection with SR 141/Peachtree Parkway. Pedestrians are permitted to cross four (4) legs of this intersections. Both roadways contain elements of commercial and residential development.

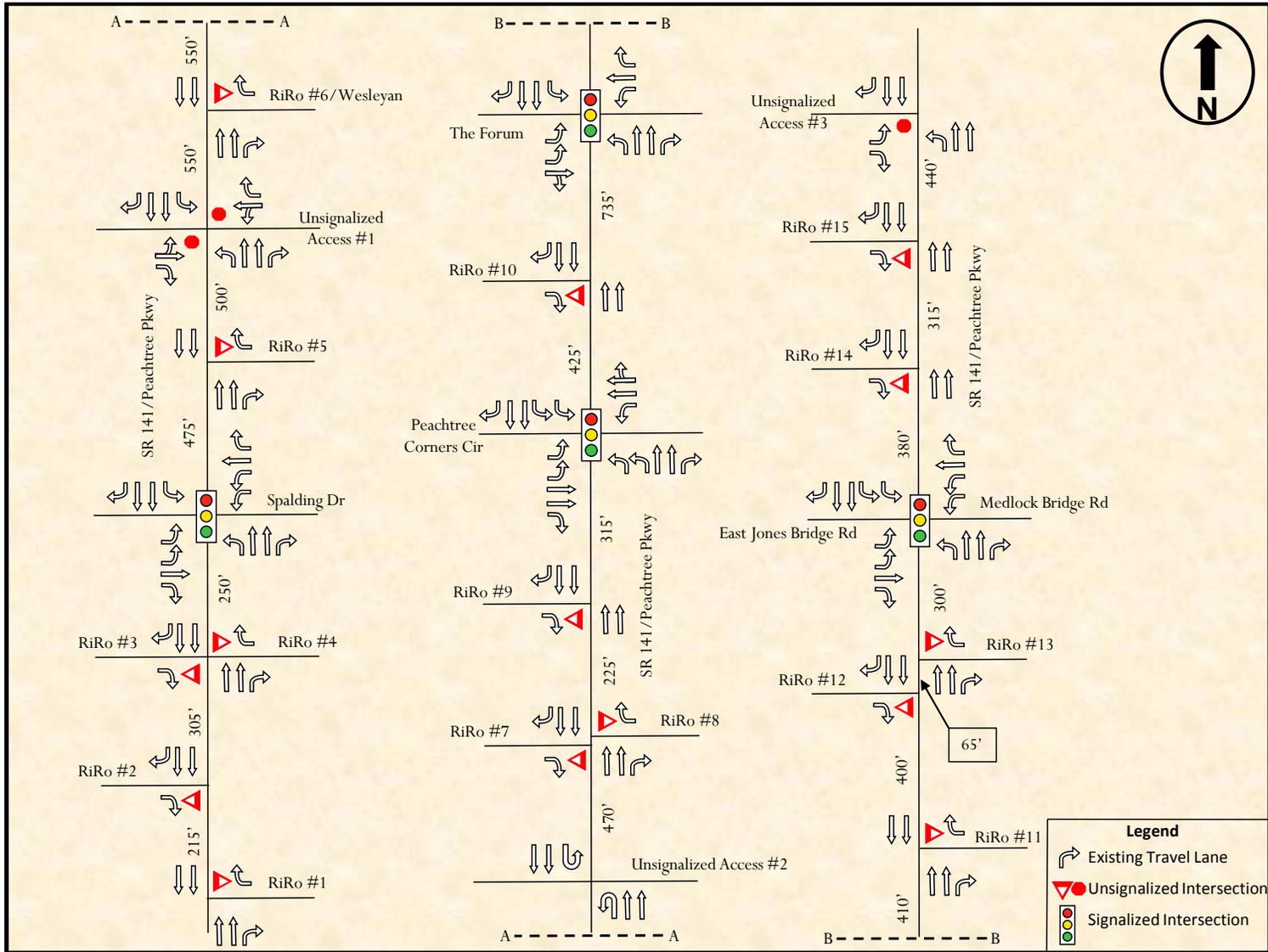
Peachtree Corners Circle is an east/west urban minor collector with a posted speed limit of 40 mph. This roadway is a four-lane divided facility on the east side of SR 141/Peachtree Parkway and a two-lane facility with a two way left turn lane (TWLTL) on the west side of SR 141/Peachtree Parkway. At the signalized intersection with SR 141/Peachtree Parkway it opens up to two through lanes in each direction with dual left

turn lanes and one right turn lane on the west side and a single left turn lane with a shared through/right turn lane on the east side of SR 141/Peachtree Parkway. Pedestrians are permitted to cross three (3) legs of this intersection. Both roadways contain elements of commercial and residential development.

The Forum Driveway is an east/west private access to the commercial development on both sides of SR 141/Peachtree Parkway. At the signalized intersection with SR 141/Peachtree Parkway the west leg has dual left turn lanes and a shared through/right turn lane. On the east leg of the intersection, the approach includes separate left, through and right turn lanes. Pedestrians are permitted to cross four (4) legs of this intersection.

East Jones Bridge/Medlock Bridge Road is an east/west urban minor arterial on Medlock Bridge Road and East Jones Bridge Road is a local street that becomes a two-lane undivided roadway west of the intersection servicing primarily residential and school traffic. The posted speed limit on both roadways is 40 mph. At the signalized intersection with SR 141/Peachtree Parkway both roadways open up to one through lane in each direction with dual left turn lanes and one right turn lane. Pedestrians are permitted to cross four (4) legs of this intersection. Both roadways contain elements of commercial and residential development.

Figure 2 – Existing Travel Lanes and Traffic Control



### 3. TRAFFIC DATA

#### SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

Turning movement counts (TMCs) were collected at the study intersections, and 24-hour directional volume counts were collected at select locations in the study area in December 2015. The existing peak hour volumes can be seen in Figure 3 and the printouts for TMCs are provided in Appendix A.

Figure 3 – Existing Peak Hour Volumes



#### Average Daily Traffic Volumes

The Build Year 2021 traffic projections were formulated for locations in the project area corresponding to the TMC locations. The future year projections based on annual growth rates were determined for the corridor. Traffic on SR 141/ Peachtree Parkway is expected to increase as a result of continuing development in the region. The local GDOT count stations and RTOP count stations were used to develop an annual growth rate that was applied to the existing traffic. The GDOT count stations at 135-0229, 1350232, 1350227, 1350436, 1350432, 1356717 and 1350341 were all considered in the calculations of generating a growth rate. All of these count stations are along the SR 141/ Peachtree Parkway corridor in the vicinity of

the study corridor. Each of the GDOT count stations contained some estimated volumes for various years. The RTOP count stations at SR 141/Peachtree Parkway at Jay Bird Alley/Technology Parkway and SR 141/Peachtree Parkway at Wellington Lake Drive/Everett Court were also used due to their high amount of reliability of counts from year to year. Using the historical traffic count data from these locations, compounded growth rate analysis was performed to help predict future traffic growth in the area. The average growth rate per year for these count locations is 1.77% per year from 2015 to 2021. Table 1 shows the GDOT and RTOP count stations considered in the calculation of growth rate for this project.

*Table 1 – Growth Rate*

GDOT Count Station ID	Location							Forecast 2021	Compounded Growth Rate/Year 2015-2021
		2010	2011	2012	2013	2014	2015		
135-0229	SR 141 south of Spalding Dr	42,170	42,120	41,890	41,890	41,900		41,301	-0.21%
1350232	SR 141 north of East Jones Bridge Rd	43,950	46,770	46,510	44,180	44,200		43,241	-0.31%
1350227	SR 141 south of Engineering Dr	42,650	42,600	44,030	44,240	50,000		59,410	2.49%
1350436	Spalding Dr west of Peachtree Corners Cir	16,750	16,140	16,050	16,130	16,100		15,055	-0.95%
1350432	Spalding Dr east of Technology Pkwy	8,750	8,740	14,090	14,160	14,200		26,676	9.43%
1356717	Peachtree Corners Cir west of Triangle Pkwy	14,590	14,120	14,100	14,420	14,400		14,254	-0.15%
1350341	Medlock Bridge Rd east of Peachtree Corners Cir	13,700	15,260	15,180	15,250	15,300		17,809	2.19%
RTOP	SR 141 @ Jaybird		45,864	44,416	46,173	47,101	47,720	51,372	1.24%
RTOP	SR 141 @ Wellington Lake Dr		51,242	53,273	54,865	55,869	55,760	63,507	2.19%
	<b>AVERAGE</b>								<b>1.77%</b>

Estimate  
No count info, used 2012 data

A Traffic Engineering Report submitted to the City of Peachtree Corners for the proposed development east of SR 141/Peachtree Parkway at The Forum was obtained. This report was generated in October 2013. This report is the only study found for any of the development proposed on the east side of SR 141/Peachtree Parkway at The Forum. This report identifies three (3) separate land uses considered; retail space, a grocery store and an apartment complex. Of those three (3) land uses considered as part of that study, two (2) of those land uses have been constructed; the retail space and grocery store. The apartment complex has yet to be constructed. As part of this project, Wolverson & Associates was tasked with applying the proposed generated traffic identified in the October 2013 Traffic Engineering Study and apply it to the models used for this project. Since two (2) of the three (3) land uses have already been constructed, only the proposed traffic generated by the proposed apartment complex will be applied to the future traffic growth applicable to this project.

The growth rate was applied to the existing TMC's in Figure 3 for the No Build scenario for Year 2021. Figure 4 shows the peak hour traffic projections with the growth rate applied as well as projected trips for the proposed apartment complex.

Figure 4 – Peak Hour Volumes (No Build) 2021



## 4. CAPACITY ANALYSIS

---

### SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

Capacity analysis were used to evaluate the projected volumes at the study intersections along the corridor. This process was used to determine the geometry and traffic control needed at each intersection to result in adequate Levels of Service (LOS) for the Build Year 2021.

*Synchro* (1) was used to conduct capacity analysis. *Synchro* implements the capacity methods of the *Highway Capacity Manual* (HCM) (2) for performing the industry standard evaluation of intersection performance.

The HCM defines LOS in terms of the amount of control delay, including initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay.

The LOS thresholds for stop controlled and signal controlled intersections are provided in Table 2.

*Table 2 – Level of Service Criteria*

LEVEL OF SERVICE	CONTROL DELAY PER VEHICLE (SEC)	
	WITH STOP-SIGN CONTROL	WITH SIGNAL CONTROL
A	$\leq 10$	$\leq 10$
B	$> 10$ and $\leq 15$	$> 10$ and $\leq 20$
C	$> 15$ and $\leq 25$	$> 20$ and $\leq 35$
D	$> 25$ and $\leq 35$	$> 35$ and $\leq 55$
E	$> 35$ and $\leq 50$	$> 55$ and $\leq 80$
F	$> 50$	$> 80$

Source: *Highway Capacity Manual*

GDOT has ranges of adequate LOS based on the area classification. Rural, sparsely developed areas have a minimum LOS requirement of C. This is due to the expectancy of rural residents for relatively uncongested conditions and to design flexibility related to lower right of way costs. The minimum LOS for urban areas is D. This reflects the greater acceptance of delay and congestion by urban residents. Additionally, the increased density of developments makes right of way costs much higher in urban areas.

### Intersection Capacity Analysis Results

#### Existing and No-Build (2021)

The study intersections were initially evaluated with the existing geometry, using the Existing Year 2016 and No-Build Year 2021 traffic volumes. This establishes a baseline for comparing improvements. The No-Build 2021 volumes include the additional traffic volumes for the proposed apartment development that is proposed to be constructed by 2021. Table 3 contains the results of the capacity analysis with the existing roadway geometry and operational conditions for the Existing Year 2016 and No-Build Year 2021. The values shown in parentheses indicate the estimated delay in seconds per vehicle.

The capacity analysis is performed using traffic volumes entered into Synchro. The LOS and Delay values generated by Synchro show poor conditions for the intersections within the study area. Even so, Synchro does not account for the additional traffic volume demand approaching these intersections during the peak hours. The TMC's are only reflective of what can be counted and processed by the existing traffic signals in the study area. Observations as well as direct involvement on the RTOP project confirms that demand far exceeds the capacity of these intersections during the peak hours. For example during the AM peak period, traffic routinely backs up from SR 141/Peachtree Parkway at Spalding Drive through these four (4) signals within this study, across the Chattahoochee River to The City of Johns Creek. During the PM peak period, traffic routinely queues from The City of Johns Creek, across the Chattahoochee River, through all four (4) signals within this study area. Although Table 3 shows poor conditions, the corridor is actually worse due to latent demand.

The Synchro printouts for the No Build Option can be seen in Appendix B.

*Table 3 – LOS and Delay Capacity Analysis: Existing and No Build 2021*

INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
	Existing 2016	No Build 2021	Existing 2016	No Build 2021
SR 141 @ Spalding Drive	F (148.8)	F (172.4)	F (116.7)	F (165.6)
SR 141 @ Peachtree Corners Circle	F (146.4)	F (165.8)	E (76.3)	F (116.7)
SR 141 @ Forum Drive	F (117.5)	F (155.0)	E (70.5)	F (128.1)
SR 141 @ East Jones Bridge Rd/Medlock Bridge Rd	F (223.3)	F (243.8)	F (117.4)	F (162.7)
Total	636.0	737.0	380.9	573.1

Build Options: Year 2021

The Build Scenarios will consist of analyzing nine (9) options.

1. Re-direct **mainline left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
2. Re-direct **side street left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
3. Re-direct **both mainline and side street left turns** from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge/Medlock Bridge Road to existing and proposed access points along SR 141/Peachtree Parkway.
4. Add **mainline through lane** of capacity along SR 141/Peachtree Parkway from Spalding Drive through East Jones Bridge/Medlock Bridge.

5. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to Spalding Drive including **additional side street through lane** of capacity and **median U-turns along mainline** of SR 141/Peachtree Parkway.
6. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to Peachtree Corners Circle including **creating a median and adding a signal on the west leg of the intersection** as well as re-routing the side street left turns.
7. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and **remove The Forum full access traffic signal** and replace with median U-turn signals.
8. Add **mainline through lane of capacity** along SR 141/Peachtree Parkway and improvements to East Jones Bridge/Medlock Bridge including **implementing a CFI** for the mainline southbound left turns and median U-turns for mainline northbound left turns.
9. **Implementing FYA signal heads** on eligible movements at signalized intersections.

### **Build Options (1 – 3)**

Build Options 1-3 analyze various combinations of median U-turn options along SR 141/Peachtree Parkway. The analysis was performed with the existing two (2) through lanes in each direction along the mainline. This was done in an effort to show how median U-turns would affect the existing geometry of the roadway. These comparisons were made so one could understand if simply adding a median U-turn operation to the existing geometry would provide an operation benefit before any additional capacity is provided along the mainline.

Consideration was given as to the type of operation of the new proposed U-turn intersections and how the re-directed left turns would make the U-turn movement. In other words, would the re-directed left turns occur at new unsignalized or signalized intersections. It was decided to assume the re-directed left turns would occur at new signalized intersections because this operation type would function the same as an unsignalized intersection in that permissive left turn movements can occur at unsignalized or signalized operation. Additionally, the signalized operation was chosen because of some known factors along the corridor in this area. During the peak hours, traffic usually queues to and from the City of Johns Creek to the north due to the extremely high traffic volumes. This occurs no matter what is happening in the City of Peachtree Corners. When traffic is queued through the intersections in this study area, the ideal operational scenario would be for the U-turn movement to be made at a traffic signal because the signal would stop the opposing traffic thus providing for an opportunity to make the U-turn movement.

In addition to re-routing the left turns to be at proposed signalized intersections, consideration was given to the capacity of the U-turn movement at the proposed signalized intersection. In other words, would the U-turn movement be a single lane or dual lane movement. Initially, the models were all created with the proposed U-turn movements occurring as single lane movements. If necessary, the storage bay for each proposed U-turn movement was modified to back up to any existing storage bays at upstream or downstream intersections. Additionally, the models that utilized single lane U-turn movements, the signal operation was set to protected-permissive so that U-turns could be made permissively when gaps in through traffic are

present. Using this approach, the models suggest that single lane U-turn movements would be acceptable at most proposed U-turn signals in most scenarios. However, the intersections where existing dual left turns with high volumes were a concern. Some instances called for increasing the capacity of the proposed U-turn movements because proposed queues in single lane U-turn movements spilled into the through lane. If the model showed U-turn queues spilling into the adjacent through lane, a dual U-turn lane was used instead of a single U-turn lane. For the models that required dual lane U-turn movements, the signal operation was set to protected-only.

A methodology for signal timing at the proposed U-turn signals was determined. The methodology was to provide the U-turn movement adequate amount of green time to allow the U-turn queue the ability to clear and not spill over into the adjacent through lane. In addition to providing enough green time for queue management, the timing of the U-turn movements, in relation to adjacent signals, was set in such a way so the through movement traffic does not queue from the downstream signal and block the U-turn movement from happening. In other words, the new U-turn signal would restrict traffic from the existing signals from queuing through the U-turn signals, thus allowing for a space to turn into. If the U-turn intersections were un-signalized, the main intersections would queue through the U-turn intersection, thus not allowing the U-turn movement to be made. In applying this methodology to the U-turn movement, the capacity analysis for the proposed U-turn signals do not show the lowest possible LOS and Delay. If one were just concerned with the lowest possible LOS and Delay for the proposed U-turn signals, the methodology for analyzing them would be geared towards lowering the green time for the U-turn movement and setting the signal relationships to accommodate the mainline traffic flow without regard to the operation of the U-turn movements. The methodology that was adopted for this study was to provide a more “real life” desired expectation of how the system would operate from a signal timing standpoint. The lowest LOS and Delay along a corridor is not necessarily the best approach. Managing queues and preventing gridlock is sometimes required during heavily congested peak periods.

Lastly, different cycle lengths were analyzed at the proposed median U-turn signals. In theory, by removing the left turn phases at these signals, a lower cycle length could be utilized. Various cycle lengths were looked at to validate the theory of being able to operate lower cycle lengths during the peak hours in the study area. Initial findings showed that lowering the cycle length provided little to no operational benefit; therefore, in-depth cycle length comparisons were not explored further.

**Build Option (1) – Mainline Left Turns Re-directed**

Build Option (1) re-directs the mainline left turn movements from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum, and East Jones Bridge/Medlock Bridge Road. Figure 5 shows how a northbound left turn movement would be re-directed. A southbound U-turn movement would occur in a similar manner. Figure 6 shows the mainline left turns will be re-directed and become U-turns at proposed signalized intersections. Figure 7 shows the re-directed volumes. The mainline left turn movements will have to continue straight through the signal they desire to turn left at and travel to the proposed U-turn traffic signal. These vehicles will make a U-turn movement either permissively or by way of a protected turn arrow. Once these vehicles make the U-turn movement, they will be allowed to travel northbound or southbound along SR 141/Peachtree Parkway and make a right turn movement to travel along the desired side street.

The analysis for Build Option (1) is performed with the existing two (2) through lanes along SR 141/Peachtree Parkway. The capacity analysis for Build Option (1) can be seen in Tables 4 and 5. Table 4 identifies the LOS and Delay comparisons and Table 5 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

The existing mainline southbound dual left turn movement at the intersection of SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge creates concern. This movement has existing high traffic volumes. The analysis suggests a mainline southbound dual U-turn movement is needed to handle this movement without spilling into the through lane. A mainline southbound dual U-turn movements is proposed at SR 141/Peachtree Parkway @ RiRo #11.

Synchro printouts for Build Option (1) can be seen in Appendix C.

**Figure 5 – Mainline Left U-Turn Movement**

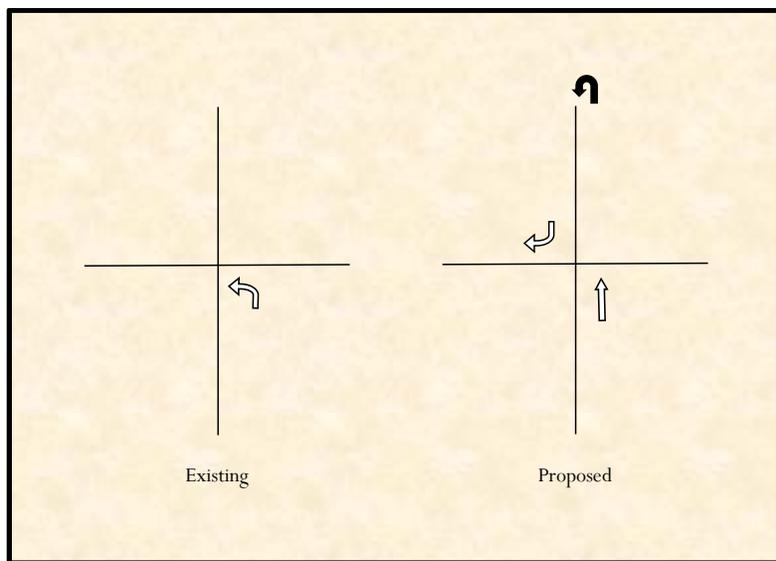


Figure 6 – Build Option (1): Mainline Left U-Turn Locations

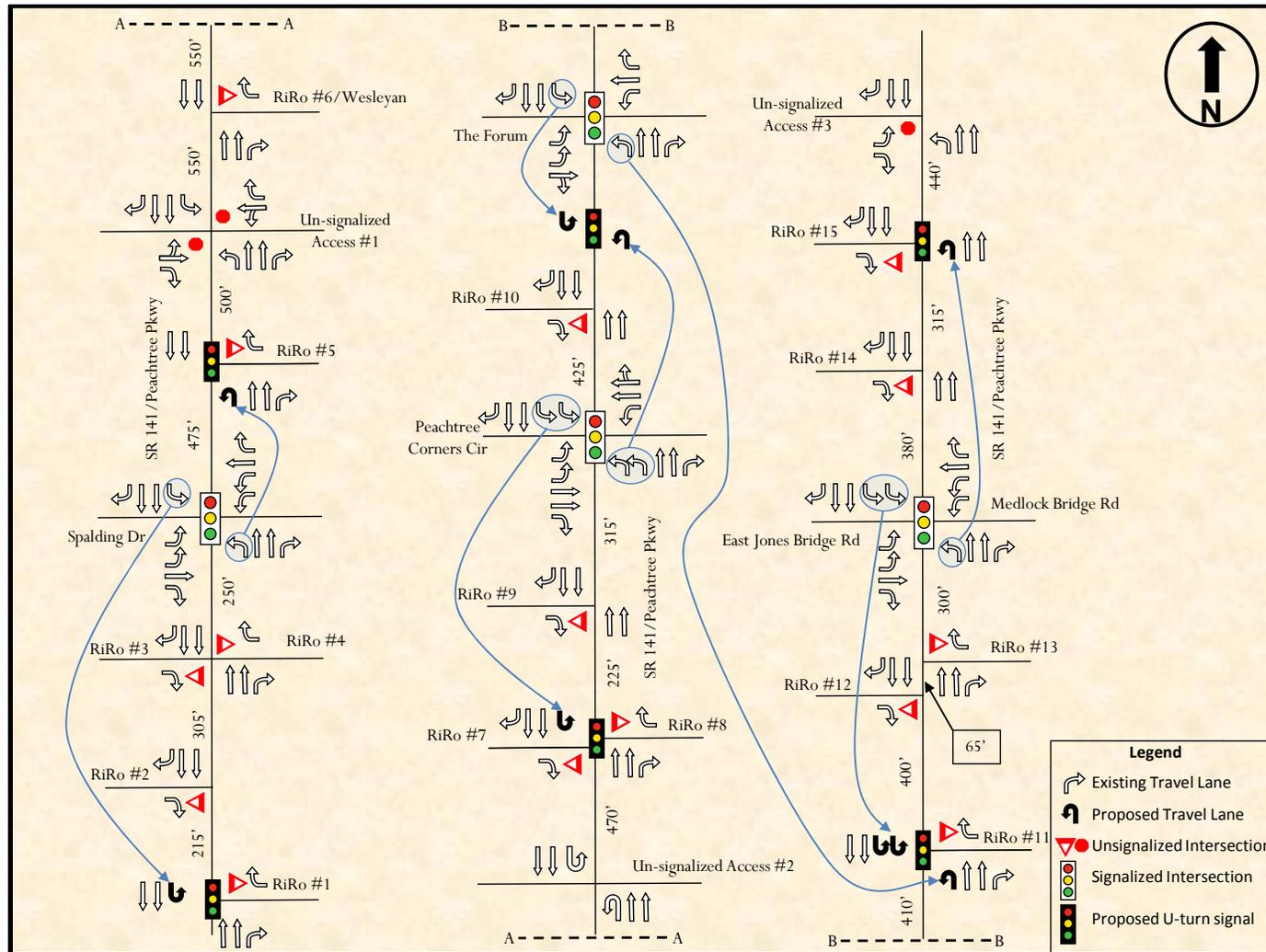


Figure 7 – Build Option (1): Mainline Left Volumes Re-Directed

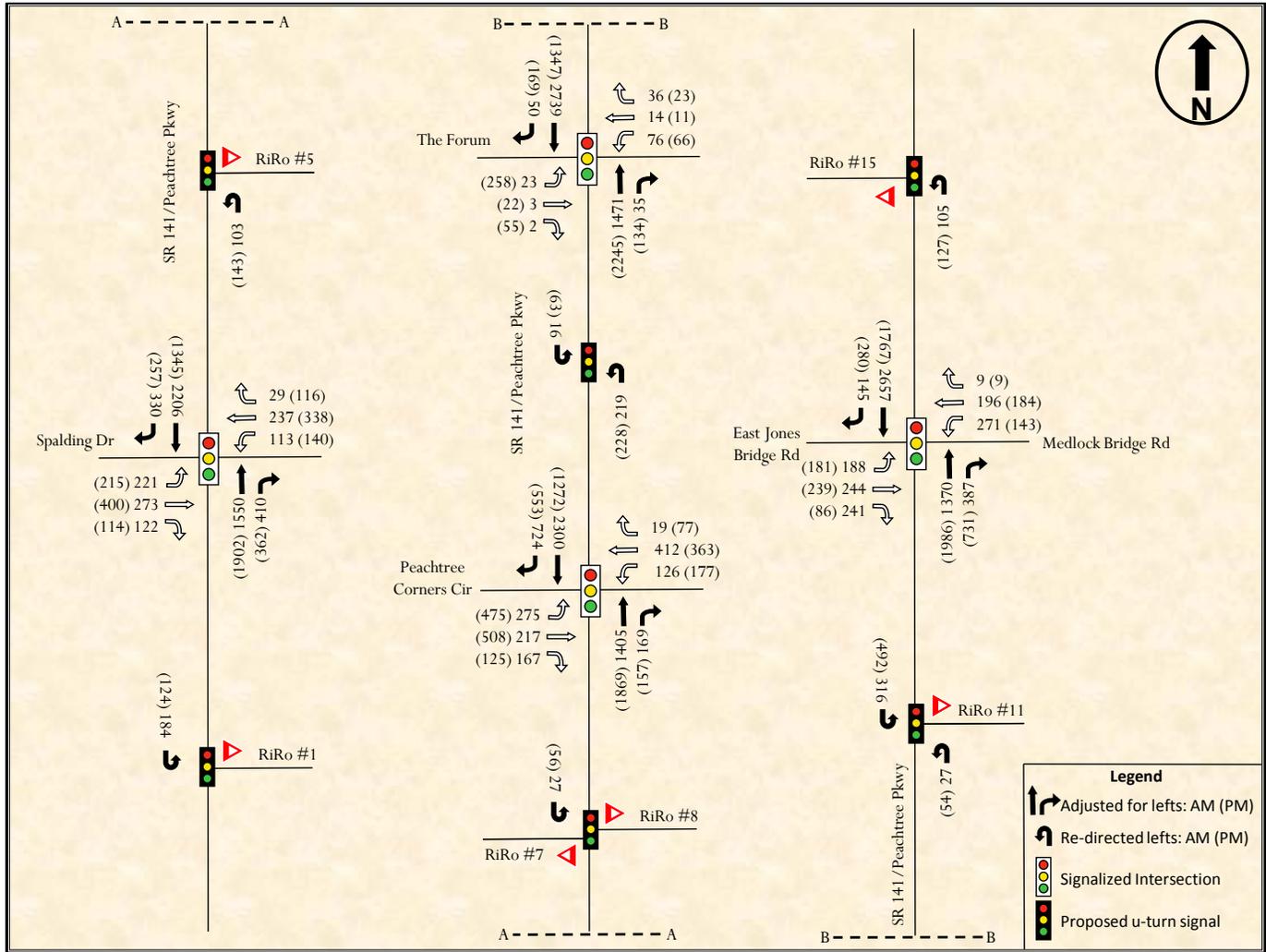


Table 4 – LOS and Delay Capacity Analysis: Build Option (1)

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		No Build 2021	Build 2021	No Build 2021	Build 2021
1	SR 141 @ RIRO #1	--	B (10.6)	--	C (34.8)
2	SR 141 @ Spalding Drive	F (172.4)	F (121.8)	F (165.6)	F (122.4)
3	SR 141 @ RIRO #5	--	E (55.6)	--	A (6.8)
4	SR 141 @ RIRO #7 & #8	--	E (77.2)	--	A (7.4)
5	SR 141 @ Peachtree Corners Circle	F (165.8)	F (126.6)	F (116.7)	F (135.0)
6	SR 141 @ Proposed U-Turn Signal	--	F (149.8)	--	C (33.9)
7	SR 141 @ Forum Drive	F (155.0)	F (117.9)	F (128.1)	E (78.0)
8	SR 141 @ RIRO #11	--	F (126.4)	--	F (102.2)
9	SR 141 @ East Jones Bridge Rd/Medlock Bridge	F (243.8)	F (193.0)	F (162.7)	F (98.2)
10	SR 141 @ RIRO #15	--	F (116.5)	--	B (14.0)
	Total	737.0	1095.4	573.1	632.7

Table 5 – Queuing Analysis: Build Option (1)

INT #	INTERSECTION	Movement	AM PEAK HOUR	PM PEAK HOUR
			Build 2021	Build 2021
1	SR 141 @ RIRO #1	SBU	m117 ft	m161 ft
3	SR 141 @ RIRO #5	NBU	m112 ft	m44 ft
4	SR 141 @ RIRO #7 & #8	SBU	25 ft	m46 ft
6	SR 141 @ Proposed U-Turn Signal	NBU	m#355	m165 ft
		SBU	25 ft	m49 ft
8	SR 141 @ RIRO #11	NBU	25 ft	25 ft
		SBU	m153 ft	m328 ft
10	SR 141 @ RIRO #15	NBU	m144 ft	m104 ft

m - volume for queue is metered by upstream signal  
# - volume exceeds capacity, queue may be longer

**Build Option (2) – Side Street Left Turns Re-directed**

Build Option (2) re-directs the Side Street left turn movements from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum, and East Jones Bridge/Medlock Bridge Road. Figure 8 shows how an eastbound left turn movement would be re-directed. The westbound U-turn movement would occur in a similar manner. Figure 9 shows the side street left turns will be re-directed and become U-turns at proposed signalized intersections. Figure 10 shows the re-directed volumes. The side street left turn movement will have to make a right turn movement approaching SR 141/Peachtree Parkway instead of a left turn movement. The desired left turning traffic will make the right turn onto SR 141/Peachtree Parkway and travel to the proposed U-turn traffic signal. This traffic will make a U-turn movement either permissively or by way of a protected turn arrow. Once these vehicles make the U-turn movement, they will be allowed to travel northbound or southbound along SR 141/Peachtree Parkway as desired.

This analysis for Build Option (2) was performed with the existing two (2) through lanes along SR 141/Peachtree Parkway. The capacity analysis for Build Option (2) can be seen in Tables 6 and 7. Table 6 identifies the LOS and Delay comparisons and Table 7 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

There is an opportunity to increase the side street through lane approach at the intersection of SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge. Due to the fact the dual side street left turns are being re-routed, the eastbound through movement was increased to be two through lanes through the intersection rather than one. The additional capacity comes from the dual left turns being re-routed. This improves the overall operation at this intersection.

None of the side street re-directed left turn movements required proposed dual U-turn movements at the proposed U-turn signal locations.

Synchro printouts for Build Option (2) can be seen in Appendix D.

***Figure 8 –Side Street Left U-Turn Movement***

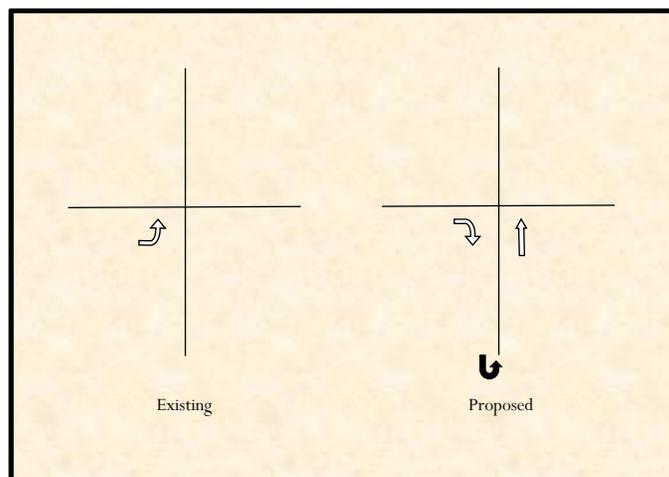


Figure 9 – Build Option (2): Side Street Left U-Turn Locations

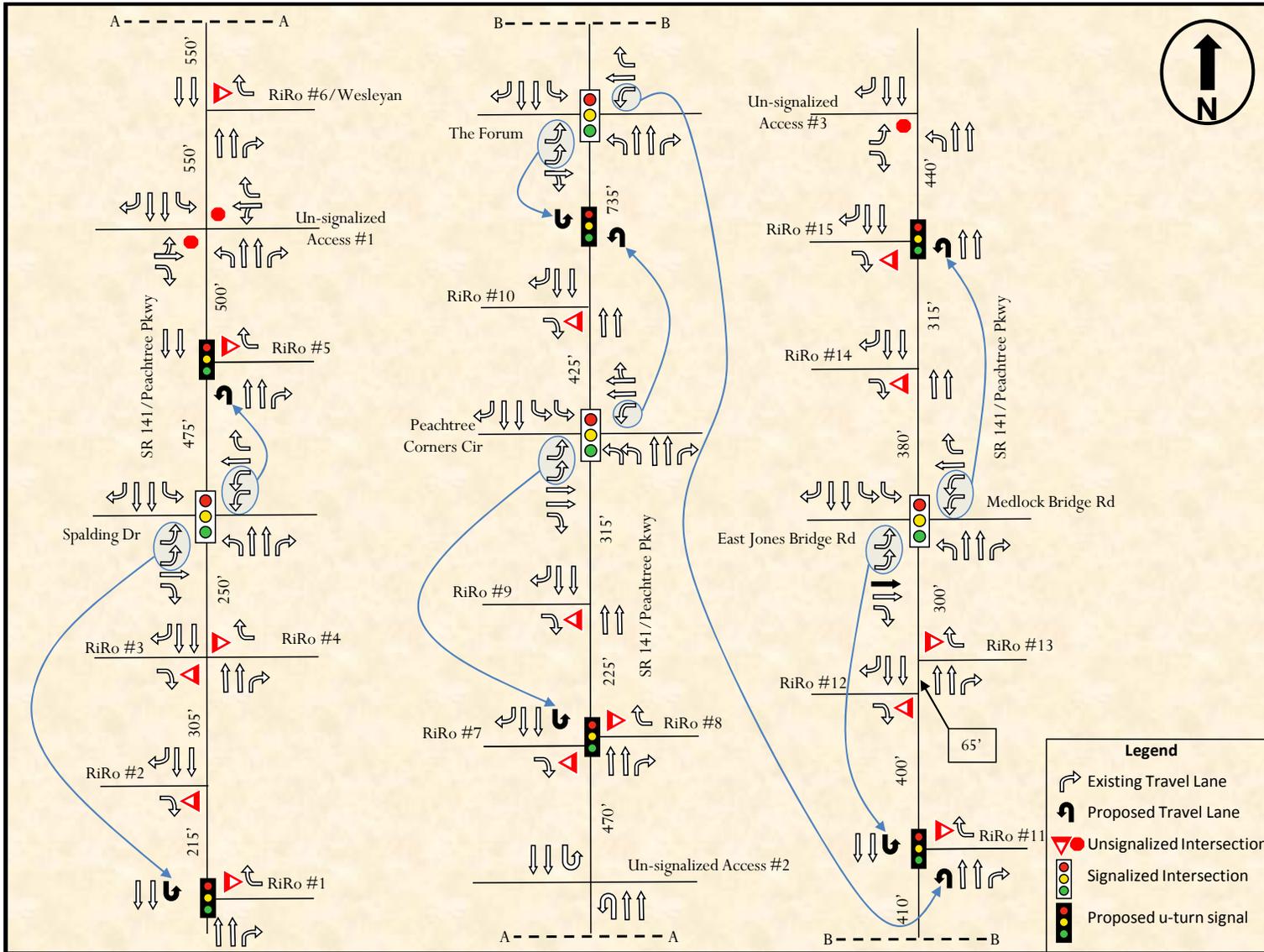


Figure 10 – Build Option (2): Side Street Left Volumes Re-Directed

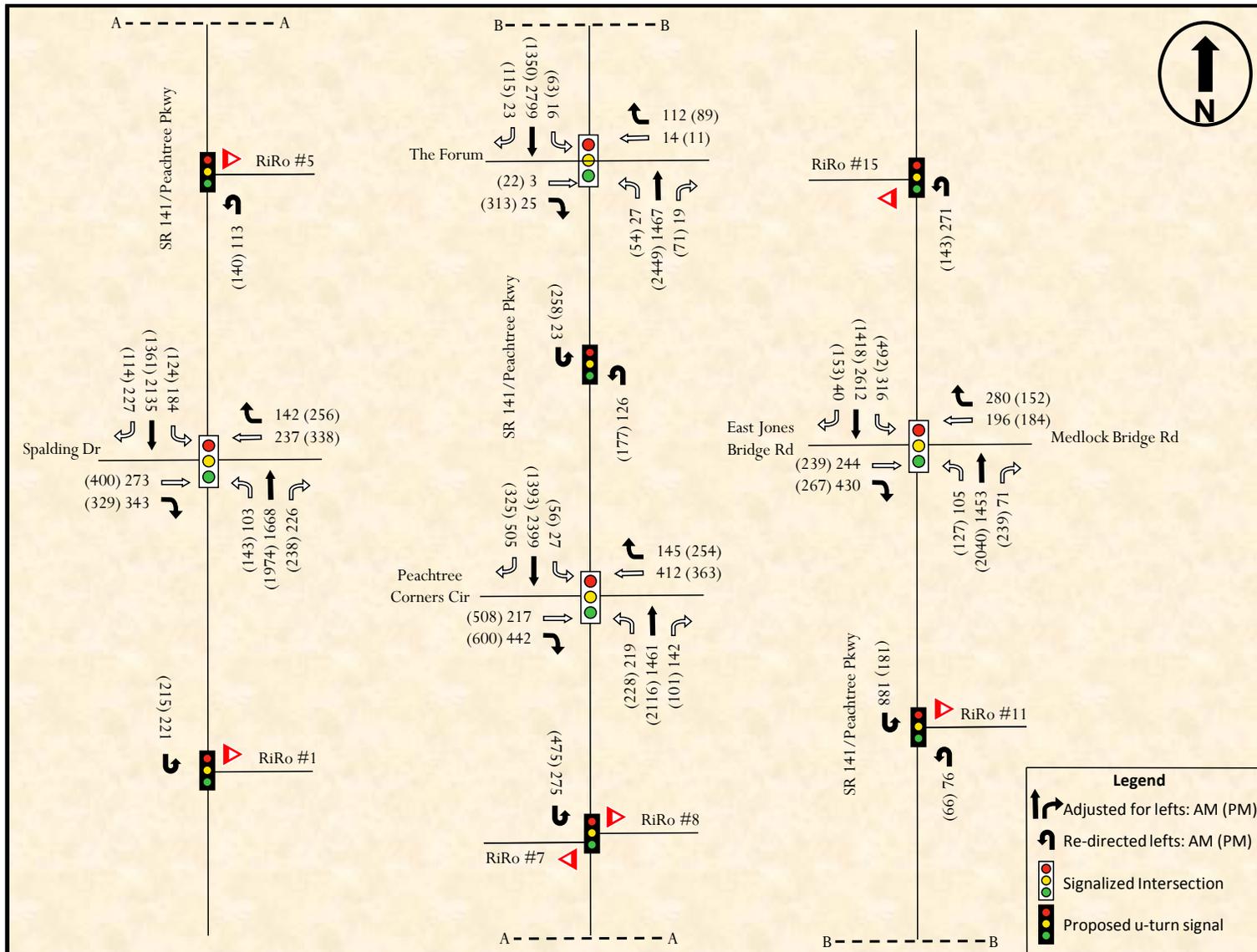


Table 6 – LOS and Delay Capacity Analysis: Build Option (2)

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		No Build 2021	Build 2021	No Build 2021	Build 2021
1	SR 141 @ RIRO #1	--	C (24.8)	--	D (51.6)
2	SR 141 @ Spalding Drive	F (172.4)	F (116.6)	F (165.6)	F (111.3)
3	SR 141 @ RIRO #5	--	E (57.7)	--	A (8.6)
4	SR 141 @ RIRO #7 & #8	--	F (84.2)	--	F (95.6)
5	SR 141 @ Peachtree Corners Circle	F (165.8)	F (141.5)	F (116.7)	F (155.2)
6	SR 141 @ Proposed U-Turn Signal	--	F (132.0)	--	E (71.0)
7	SR 141 @ Forum Drive	F (155.0)	F (143.4)	F (128.1)	F (173.3)
8	SR 141 @ RIRO #11	--	F (136.8)	--	D (44.3)
9	SR 141 @ East Jones Bridge Rd/Medlock Bridge	F (243.8)	F (188.8)	F (162.7)	F (116.6)
10	SR 141 @ RIRO #15	--	F (169.8)	--	B (16.4)
	Total	737.0	1195.6	573.1	843.9

Table 7 – Queuing Analysis: Build Option (2)

INT #	INTERSECTION	Movement	AM PEAK HOUR	PM PEAK HOUR
			Build 2021	Build 2021
1	SR 141 @ RIRO #1	SBU	m#180 ft	m#300 ft
3	SR 141 @ RIRO #5	NBU	#159 ft	m128 ft
4	SR 141 @ RIRO #7 & #8	SBU	m89 ft	m274 ft
6	SR 141 @ Proposed U-Turn Signal	NBU	m114 ft	m151 ft
		SBU	25 ft	m103 ft
8	SR 141 @ RIRO #11	NBU	85 ft	43 ft
		SBU	m86 ft	m#204 ft
10	SR 141 @ RIRO #15	NBU	m188 ft	#128 ft

m - volume for queue is metered by upstream signal  
# - volume exceeds capacity, queue may be longer

### **Build Option (3) – Mainline and Side Street Left Turns Re-directed**

Build Option (3) combines Build Options (1) and (2) re-directing the mainline and side street left turn movements from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, The Forum, and East Jones Bridge/Medlock Bridge Road. Figure 11 shows how a northbound and eastbound left turn movement would be re-directed. The southbound and westbound U-turn movements would occur in a similar manner. Figure 12 shows the mainline and side street left turns will be re-directed and become U-turns at proposed signalized intersections. Figure 13 shows the re-directed volumes.

The mainline left turn movements will continue straight through the signal they desire to turn left at and travel to the proposed U-turn traffic signal. These vehicles will make a U-turn movement either permissively or by way of a protected turn arrow. Once these vehicles make the U-turn movement, they will be allowed to travel northbound or southbound along SR 141/Peachtree Parkway and make a right turn movement to travel along the desired side street.

The side street left turn movement will have to make a right turn movement approaching SR 141/Peachtree Parkway instead of a left turn movement. The desired left turning traffic will make the right turn onto SR 141/Peachtree Parkway and travel to the proposed U-turn traffic signal. This traffic will make a U-turn movement either permissively or by way of a protected turn arrow. Once these vehicles make the U-turn movement, they will be allowed to travel northbound or southbound along SR 141/Peachtree Parkway as desired.

This analysis for Build Option (3) was performed with the existing two (2) through lanes along SR 141/Peachtree Parkway. The capacity analysis for Build Option (3) can be seen in Tables 8 and 9. Table 8 identifies the LOS and Delay comparisons and Table 9 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

The existing mainline southbound dual left turn movements at the intersection of SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge and Peachtree Corners Circle creates concern. These movement are being tasked with handling high left turning volumes in the proposed conditions. The analysis suggests a mainline southbound dual U-turn movement is needed to handle this movement without spilling into the through lane. Mainline southbound dual U-turn movements are proposed at SR 141/Peachtree Parkway @ RiRo #11 and SR 141/Peachtree Parkway @ RiRo #7.

There is an opportunity to increase the side street through lane approach at the intersection of SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge. Due to the fact the dual side street left turns are being re-routed, the eastbound through movement was increased to be two through lanes through the intersection rather than one. The additional capacity comes from the dual left turns being re-routed. This improves the overall operation at this intersection

Synchro printouts for Build Option (3) can be seen in Appendix E.

*Figure 11 – Mainline and Side Street Left U-Turn Movement*

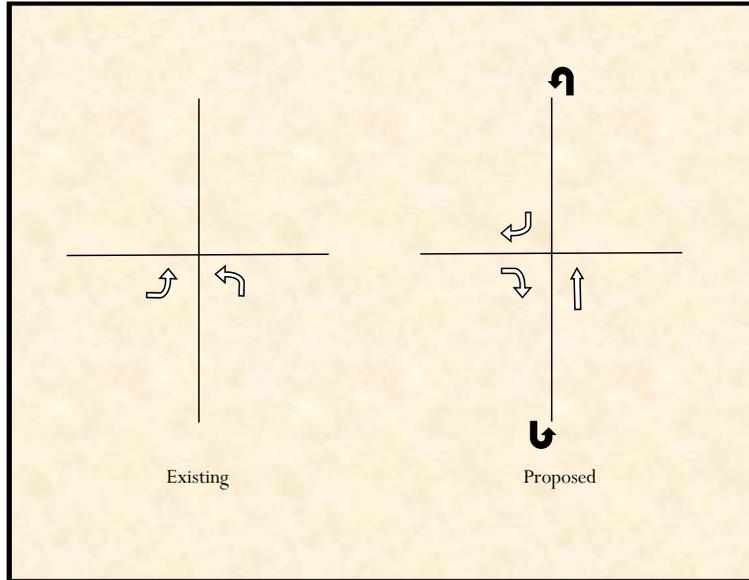


Figure 12 – Build Option (3): Mainline and Side Street Left U-Turn Locations

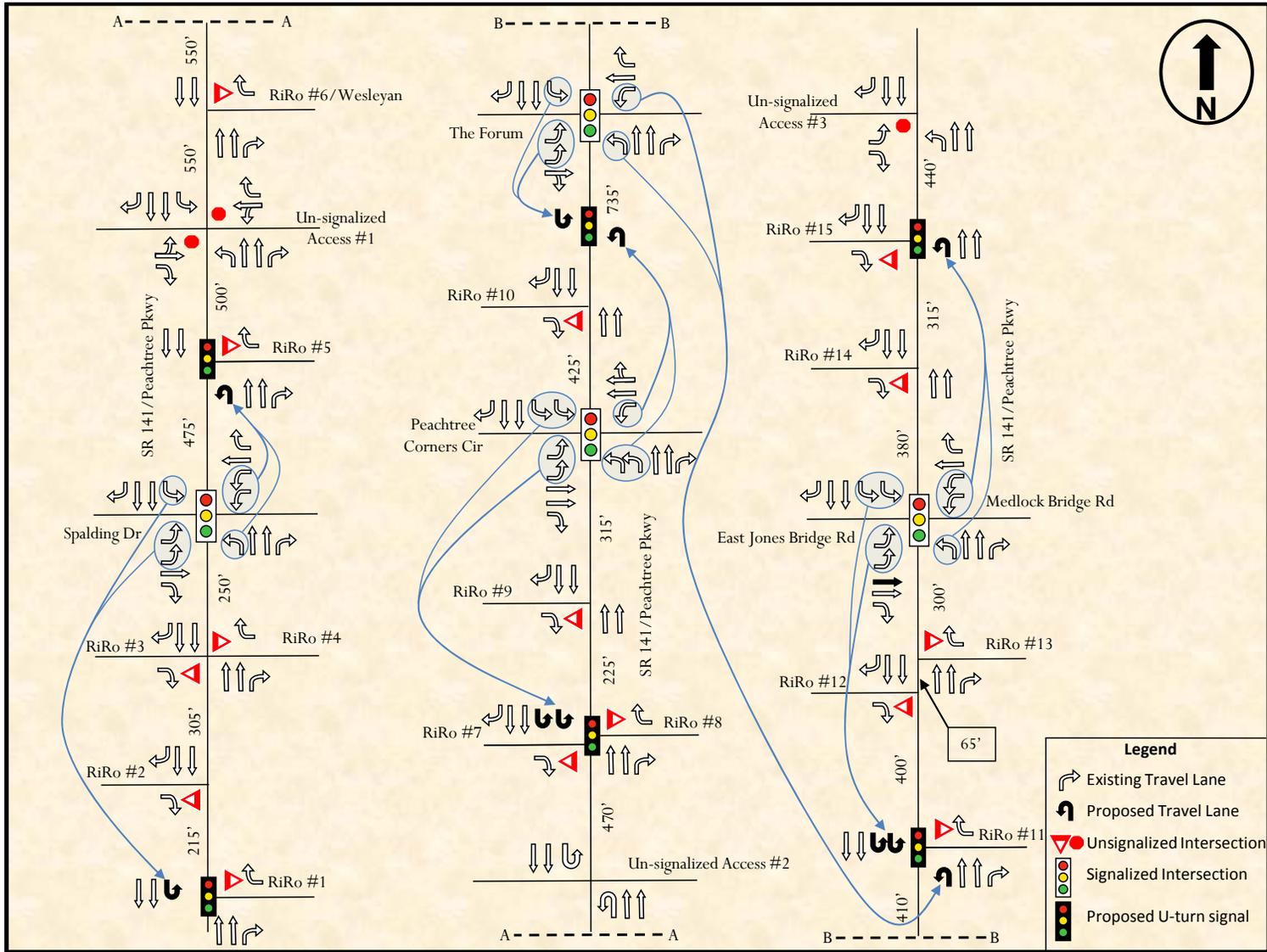


Figure 13 – Build Option (3): Mainline and Side Street Left Volumes Re-Directed

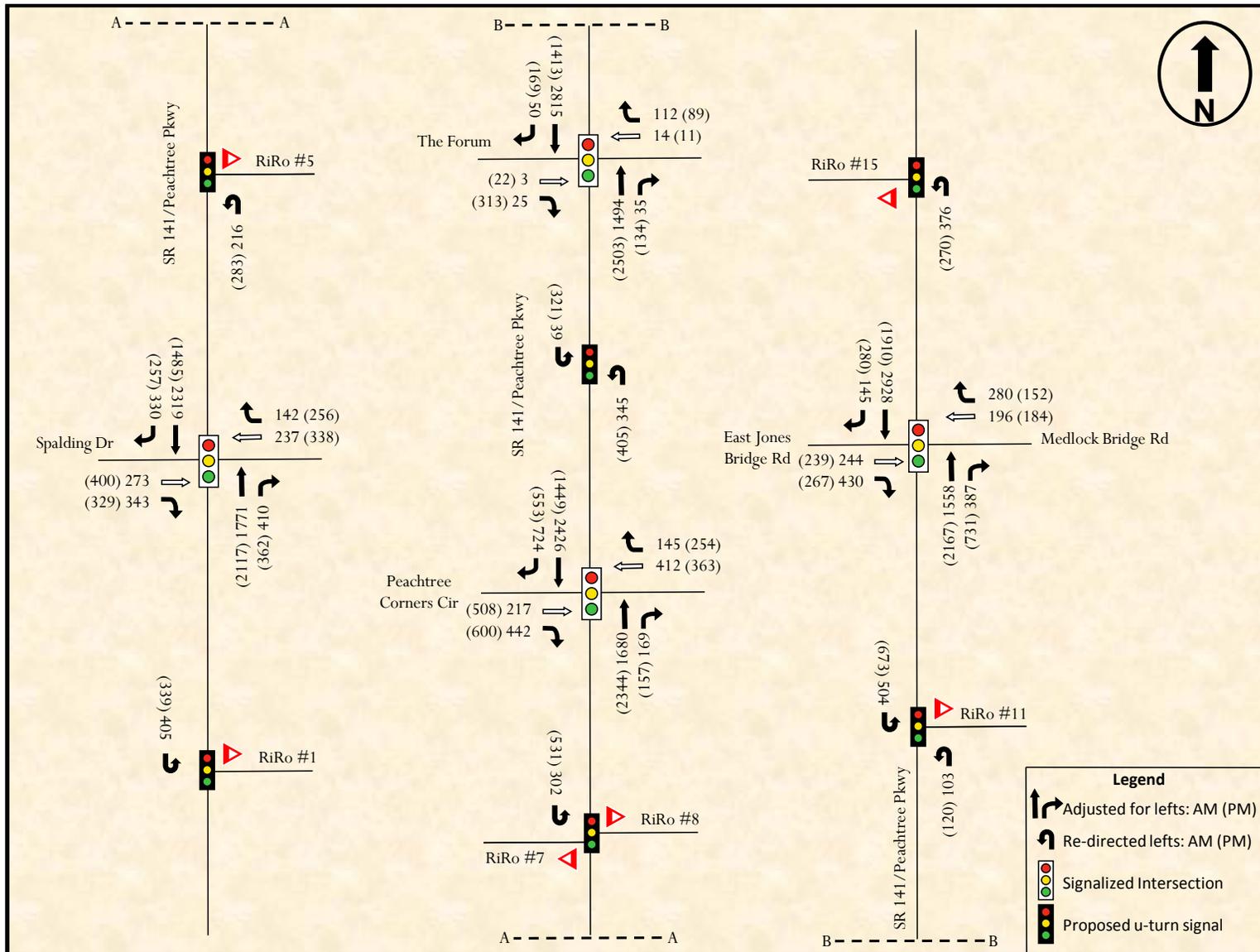


Table 8 – LOS and Delay Capacity Analysis: Build Option (3)

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		No Build 2021	Build 2021	No Build 2021	Build 2021
1	SR 141 @ RIRO #1	--	D (35.5)	--	F (102.2)
2	SR 141 @ Spalding Drive	F (172.4)	F (103.3)	F (165.6)	F (81.1)
3	SR 141 @ RIRO #5	--	F (95.4)	--	B (16.4)
4	SR 141 @ RIRO #7 & #8	--	F (85.2)	--	E (58.9)
5	SR 141 @ Peachtree Corners Circle	F (165.8)	F (107.7)	F (116.7)	F (94.5)
6	SR 141 @ Proposed U-Turn Signal	--	F (205.1)	--	F (106.3)
7	SR 141 @ Forum Drive	F (155.0)	F (115.5)	F (128.1)	F (131.5)
8	SR 141 @ RIRO #11	--	F (140.5)	--	F (105.9)
9	SR 141 @ East Jones Bridge Rd/Medlock Bridge	F (243.8)	F (188.2)	F (162.7)	F (82.3)
10	SR 141 @ RIRO #15	--	F (209.0)	--	C (33.6)
	Total	737.0	1285.4	573.1	812.7

Table 9 – Queuing Analysis: Build Option (3)

INT #	INTERSECTION	Movement	AM PEAK HOUR	PM PEAK HOUR
			Build 2021	Build 2021
1	SR 141 @ RIRO #1	SBU	m#362 ft	m#499
3	SR 141 @ RIRO #5	NBU	m239 ft	m#215
4	SR 141 @ RIRO #7 & #8	SBU	m170 ft	m300 ft
6	SR 141 @ Proposed U-Turn Signal	NBU	m#446 ft	m#367 ft
		SBU	25 ft	m#461 ft
8	SR 141 @ RIRO #11	NBU	m#178 ft	25 ft
		SBU	m299 ft	m#435 ft
10	SR 141 @ RIRO #15	NBU	m#583 ft	m#273 ft

m - volume for queue is metered by upstream signal  
 # - volume exceeds capacity, queue may be longer

## Build Options (1-3) Median U-Turn Comparisons

The following tables compare the results of the capacity analysis for the **through movements only**. Table 10 compares the southbound through movement and Table 11 compares the northbound through movement. The values shown in Tables 10 and 11 are Delay in seconds per vehicle. The total at the bottom summarizes the delay for each intersection to provide a total delay for each Option.

*Table 10 – Southbound Comparison*

Intersection	Southbound Through Movement				Southbound Through Movement			
	* AM Peak Period				PM Peak Period			
	No Build	Option 1	Option 2	Option 3	No Build	Option 1	Option 2	Option 3
SR 141 @ RIRO #1	0	0	0	0	0	0	0	0
SR 141 @ Spalding Drive	97.3	74.4	58.2	21.5	2.5	0.0	24.9	2.2
SR 141 @ RIRO #5	0	4.4	0.3	15.0	0	8.2	9.6	2.7
SR 141 @ RIRO #7 & #8	0	0	0	0	0	0	0	0
SR 141 @ Peachtree Corners Circle	92.1	29.5	56.1	14.7	1.7	46.0	18.6	0.5
SR 141 @ Proposed U-Turn Signal	0	50.3	29.4	123.7	0	2.6	2.7	8.3
SR 141 @ Forum Drive	59.8	9.1	30.6	4.0	0.7	0.6	0.6	0.6
SR 141 @ RIRO #11	0	11.6	33.6	13.0	0	2.8	3.0	1.9
SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	164.8	87.3	82.8	77.6	23.0	33.1	13.4	9.7
SR 141 @ RIRO #15	0	29.6	90.4	129.5	0	6.9	34.2	11.4
<b>Total</b>	414.0	296.2	381.4	399.0	27.9	100.2	107.0	37.3

\* indicates progression direction during peak period

*Table 11 – Northbound Comparison*

Intersection	Northbound Through Movement				Northbound Through Movement			
	AM Peak Period				* PM Peak Period			
	No Build	Option 1	Option 2	Option 3	No Build	Option 1	Option 2	Option 3
SR 141 @ RIRO #1	0	5.8	8.5	5.3	0	8.5	6.0	37.4
SR 141 @ Spalding Drive	36.5	18.5	50.6	12.8	71.1	30.0	7.3	3.7
SR 141 @ RIRO #5	0	0	0	0	0	0	0	0
SR 141 @ RIRO #7 & #8	0	5.6	20.4	16.2	0	2.2	12.5	8.6
SR 141 @ Peachtree Corners Circle	0.6	30.2	26.0	43.1	2.3	63.9	104.5	20.2
SR 141 @ Proposed U-Turn Signal	0	6.5	1.7	3.3	0	5.0	7.7	10.9
SR 141 @ Forum Drive	0.8	0.7	0.7	0.6	5.5	2.6	3.2	2.5
SR 141 @ RIRO #11	0	1.9	4.0	8.6	0	29.5	2.7	20.6
SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	2.3	27.9	0.5	0.9	19.1	16.6	10.7	9.6
SR 141 @ RIRO #15	0	0	0	0	0	0	0	0
<b>Total</b>	40.2	97.1	112.4	90.8	98.0	158.3	154.6	113.5

\* indicates progression direction during peak period

The “0” values indicate directions at intersections where through movements are free flow. When comparing Build Options (1), (2) and (3) to each other, Table 10 shows in the southbound direction, Option (1) provides the best results in the AM Peak Period in the progression direction. In Table 11,

Option (3) shows the best results in the PM Peak Period in the progression direction. It is evident that Option (2) does not provide the best overall results for any direction in any peak period.

An additional comparison was made. Because the through movement volumes are dramatically different depending on direction during a given peak period, weighted calculations were made to compare all the intersections within the study area with through movement volumes included. This was done by summarizing the total delay for each Option, see bottom of Tables 10 and 11, then multiplying by through volumes for all intersections in each direction in each peak period. Those values were summarized then an average delay was calculated by dividing out the summary of the volumes in each direction in each peak period for all intersections. The values seen below in Table 12 shows an average delay of each through vehicle, in seconds, for all directions in both peak periods.

*Table 12 – Overall Through Movement Delay Comparison*

	No Build	Option 1	Option 2	Option 3
Avg	177.6	181.1	212.1	189.1

## Build Options (4 – 9)

Build Options 4-9 were analyzed with three (3) through lanes along the mainline. It is anticipated that three (3) through lanes will eventually be constructed along this corridor at some point in the future regardless of any additional improvements being implemented. Since it is expected that three (3) through lanes will eventually be constructed at some point, Build Options 5-8 were compared to Build Option (4) in their respective capacity analysis tables.

### Build Option (4) – 3<sup>rd</sup> Through lane along SR 141/ Peachtree Parkway

Build Option (4) studies an additional lane of capacity along the mainline, SR 141/ Peachtree Parkway. Currently two through lanes are present along the mainline and during the peak hours, volumes far exceed the through lane capacity of the roadway. It is expected that eventually an additional through lane of capacity will be constructed along SR 141/ Peachtree Parkway in the future. This analysis compares the No Build 2021 conditions to the future conditions in 2021. The capacity analysis for Build Option (4) can be seen in Tables 13 and 14. Table 13 identifies the LOS and Delay comparisons and Table 14 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

**Table 13 – LOS and Delay Capacity Analysis: Build Option (4)**

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		No Build 2021	Build 2021	No Build 2021	Build 2021
2	SR 141 @ Spalding Drive	F (172.4)	E (62.5)	F (165.6)	F (92.4)
5	SR 141 @ Peachtree Corners Circle	F (165.8)	E (68.4)	F (116.7)	E (64.0)
7	SR 141 @ Forum Drive	F (155.0)	C (24.5)	F (128.1)	C (20.7)
9	SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	F (243.8)	F (120.3)	F (162.7)	F (86.5)
	Total	736.2	275.7	573.1	263.6

**Table 14 – Queuing Analysis: Build Option (4)**

INT #	INTERSECTION	Movement	AM PEAK HOUR		PM PEAK HOUR	
			No Build 2021	Build 2021	No Build 2021	Build 2021
2	SR 141 @ Spalding Drive	NB	#1076 ft	517 ft	#1442 ft	694 ft
		SB	~1947 ft	m229 ft	m#846 ft	486 ft
5	SR 141 @ Peachtree Corners Circle	NB	m677 ft	529 ft	m534 ft	271 ft
		SB	m#1042 ft	m#1015 ft	#612 ft	117 ft
7	SR 141 @ Forum Drive	NB	m556 ft	m267 ft	m#1531 ft	m#1056 ft
		SB	m#1320 ft	m#1143 ft	#679 ft	109 ft
9	SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	NB	552 ft	272 ft	m106 ft	m410 ft
		SB	#2137 ft	#1208 ft	757 ft	395 ft

m - volume for queue is metered by upstream signal  
 # - volume exceeds capacity, queue may be longer  
 ~ - volume exceeds capacity, queue theoretically infinite

**Build Option (5) – Operational Improvements to Spalding Drive**

Build Option (5) studies operational improvements to SR 141/Peachtree Parkway @ Spalding Drive. During the peak hours, the delay and queuing along Spalding Drive is extreme. The volumes entered into the Synchro models only capture what can be counted. The vehicular demand far exceeds what can be processed at the traffic signal. This operational improvement includes implementing median U-turn traffic signals along SR 141/Peachtree Parkway to re-direct the mainline northbound and southbound left turns as well as an additional eastbound and westbound through lane of capacity along Spalding Drive. The additional through lane along Spalding Drive would carry two lanes, instead of the existing one lane, for both the eastbound and westbound directions. The additional through lane of capacity along Spalding Drive would drop into the existing one lane of capacity at a logical point downstream of the signal. This analysis compares Build Option (4) to the operational improvements mentioned here. Figure 14 shows the studied improvement to the operation of Spalding Drive.

The capacity analysis for Build Option (5) can be seen in Tables 15 and 16. Table 15 identifies the LOS and Delay comparisons and Table 16 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

**Figure 14 – Build Option (5): Spalding Drive Operational Improvements**

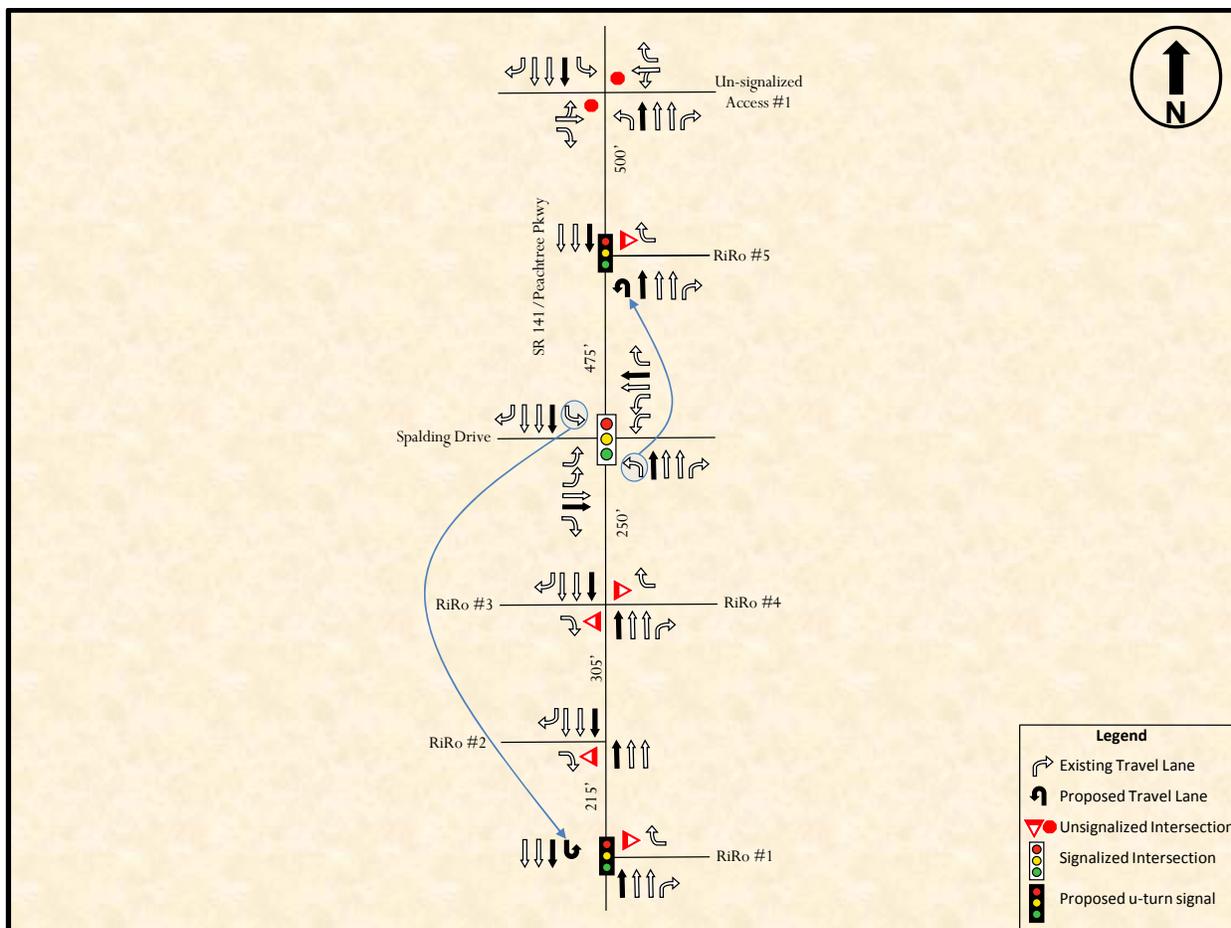


Table 15 – LOS and Delay Capacity Analysis: Build Option (5)

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
1	SR 141 @ RIRO #1	--	A (9.5)	--	B (12.1)
2	SR 141 @ Spalding Drive	E (62.5)	C (24.4)	F (92.4)	E (57.0)
3	SR 141 @ RIRO #5	--	A (7.9)	--	A (8.7)
	Total	62.5	41.8	92.4	77.8

Table 16 – Queuing Analysis: Build Option (5)

INT #	INTERSECTION	Movement	AM PEAK HOUR		PM PEAK HOUR	
			(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
2	SR 141 @ Spalding Drive	EB	#572 ft	202 ft	#879 ft	285 ft
		WB	#468 ft	176 ft	#711 ft	250 ft

# - volume exceeds capacity, queue may be longer

### **Build Option (6) – Operational Improvements to Peachtree Corners Circle**

Build Option (6) studies operational improvements to SR 141/Peachtree Parkway @ Peachtree Corners Circle. There are several existing issues that are present with the operation at and near this existing signalized intersection. First, especially during the PM Peak Hour, the eastbound traffic volumes are very high. Specifically the eastbound left turn onto SR 141/Peachtree Parkway. An operational change was studied to eliminate the eastbound and westbound left turn movements, see Figure 15, and re-route them through the intersection. The re-routed eastbound left turning traffic would be allowed to make a left turn movement at the intersection of Medlock Bridge Road @ Peachtree Corners Circle. Improvements to that intersection would need to be made due to the amount of turning vehicles that would be added. A new traffic signal or a roundabout could be considerations for improved operation at that intersection. Additionally, it was studied to re-route the westbound left turning traffic to a new traffic signal at The Forum and this movement would be allowed to make a U-turn movement then a subsequent right turn to access SR 141/Peachtree Parkway southbound.

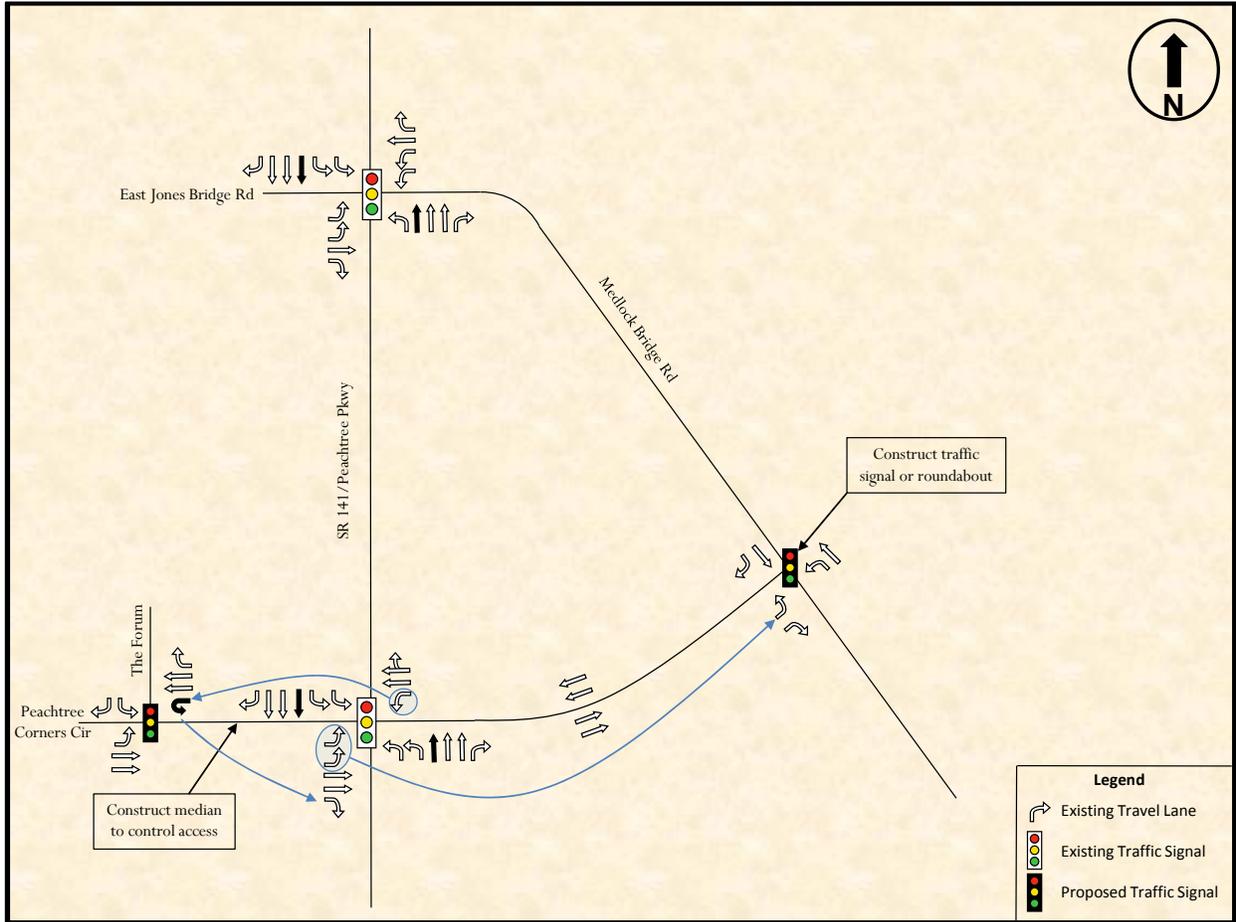
Secondly, operational issues exist along Peachtree Corners Circle at the unsignalized intersection with The Forum. When eastbound traffic queues along Peachtree Corners Circle from SR 141/Peachtree Parkway, it blocks the access to the unsignalized intersection. Specifically, vehicles wishing to turn left out of The Forum are trapped and cannot make the maneuver. Those vehicles that do try to make the maneuver end up blocking the westbound through lanes on Peachtree Corners Circle. By adding a traffic signal at this access point and installing a median along Peachtree Corners Circle in this section, not only can the vehicles exiting The Forum turn left with a protected movement, westbound left turning traffic at SR 141/Peachtree Parkway can become through vehicles and then make a U-turn movement at the proposed traffic signal.

Due to Build Option (1) not showing significant improvement to add median U-turn traffic signals at SR 141/Peachtree Parkway at Peachtree Corners Circle to re-route the mainline northbound and southbound left turning traffic, that was not included as part of this particular analysis for Build Option (6).

Lastly, a 3<sup>rd</sup> through lane of capacity along SR 141/Peachtree Parkway was also included in the analysis for this improvement as previously stated. Figure 15 shows the studied improvement to the operation of Peachtree Corners Circle.

The capacity analysis for Build Option (6) can be seen in Tables 17 and 18. Table 17 identifies the LOS and Delay comparisons and Table 18 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

Figure 15 – Build Option (6): Peachtree Corners Circle Operational Improvements



*Table 17 – LOS and Delay Capacity Analysis: Build Option (6)*

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
5	SR 141 @ Peachtree Corners Circle	E (68.4)	D (50.3)	E (64.0)	E (55.0)

*Table 18 – Queuing Analysis: Build Option (6)*

INT #	INTERSECTION	Movement	AM PEAK HOUR		PM PEAK HOUR	
			(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
5	SR 141 @ Peachtree Corners Circle	EB	167 ft	296 ft	#494 ft	m404 ft
		WB	#381 ft	375 ft	#397 ft	381 ft

m - volume for queue is metered by upstream signal  
# - volume exceeds capacity, queue may be longer

### **Build Option (7) – Operational Improvements at The Forum**

Identical to Build Option (1), Build Option (7) re-directs the mainline left turn movements from SR 141/Peachtree Parkway @ Spalding Drive, Peachtree Corners Circle, and East Jones Bridge/Medlock Bridge Road. However, in combination with those operational changes from Build Option (1), the full access traffic signal at The Forum would be removed and a right-in right-out intersection would be constructed in its place in Build Option (7). One of the negative aspects of Build Options (1-3) is the proposed median U-turn signal spacing along SR 141/Peachtree Parkway between Peachtree Corners Circle and East Jones Bridge/Medlock Bridge Road. If the full access traffic signal at The Forum would remain, the spacing of the proposed median U-turn signals would be very close to the adjacent full access signals, which in turn creates operational, geometric and signal timing challenges. If the full access traffic signal at The Forum were modified to a RiRo access and the U-turn traffic signals were positioned at a more ideal spacing, this would allow those U-turn movement to occur and not back into the adjacent through lanes.

To maintain the “left out” movement from both sides of The Forum, motorists would make a “right out” then make an immediate U-turn at the proposed U-turn signal.

Just like Figure 6, Figure 16 shows where all of the mainline median U-turn movements would occur. However, the difference shown in Figure 16 is the existing full access traffic signal at The Forum becomes a RiRo and the spacing of the proposed U-turn signals is more advantageous to the proximity to the adjacent intersections.

The capacity analysis for Build Option (7) can be seen in Table 19. Table 19 identifies the LOS and Delay comparisons. Given this operational improvements removes a signal and the before condition includes the signal, the queuing comparisons do not show a relative comparison.

Figure 16 – Build Option (7): The Forum Operational Improvements

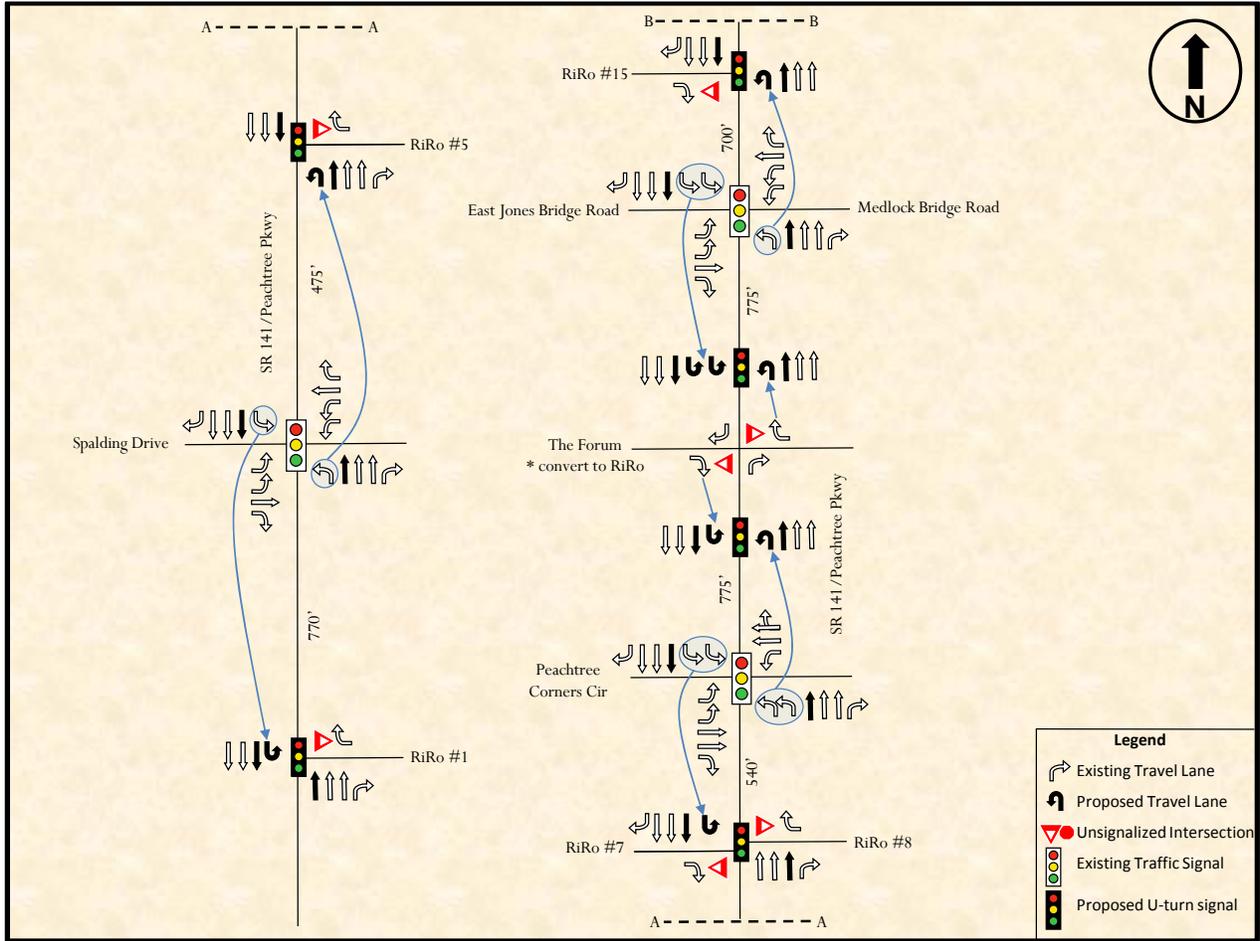


Table 19 – LOS and Delay Capacity Analysis: Build Option (7)

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
1	SR 141 @ RIRO #1	--	A (7.1)	--	A (7.8)
2	SR 141 @ Spalding Drive	E (62.5)	C (30.4)	F (92.4)	E (58.3)
3	SR 141 @ RIRO #5	--	A (5.0)	--	A (2.8)
4	SR 141 @ RIRO #7 & #8	--	A (1.1)	--	A (2.9)
5	SR 141 @ Peachtree Corners Circle	E (68.4)	D (40.7)	E (64.0)	D (51.5)
6	SR 141 @ Proposed U-Turn Signal	--	C (27.4)	--	C (32.2)
7	SR 141 @ Forum Drive	C (24.5)	--	C (20.7)	--
8	SR 141 @ RIRO #11	--	A (9.5)	--	B (18.0)
9	SR 141 @ East Jones Bridge Rd/Medlock Bridge Rd	F (120.3)	E (74.4)	F (86.5)	E (69.9)
10	SR 141 @ RIRO #15	--	B (13.4)	--	A (5.3)
	Total	275.7	209.0	263.6	248.7

**Build Option (8) – Operational Improvements at East Jones Bridge/Medlock Bridge Road**

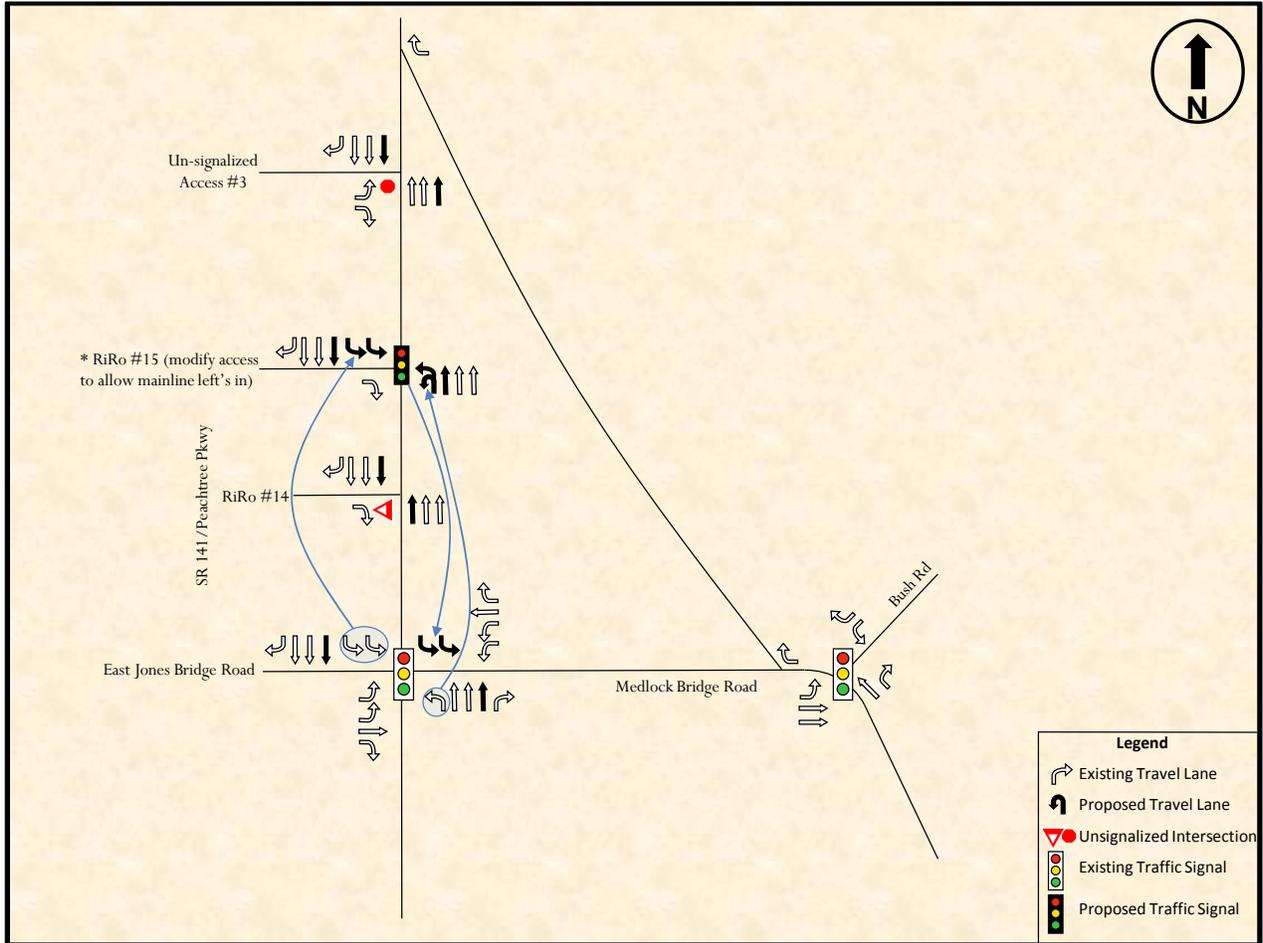
Build Option (8) studies operational improvements to SR 141/Peachtree Parkway @ East Jones Bridge/Medlock Bridge Road. There are existing issues present during the Peak Hours at this signalized intersection. During the Peak Periods, the northbound and southbound left turning traffic volumes are very high, often spilling into the adjacent through lanes. Currently, both movements are protected-only signalized left turns. The suggested improvement would be to create a Continuous Flow Intersection (CFI) for the southbound left turn movement and a median U-turn for the northbound left turn movement. Figure 17 shows the proposed operation of this improvement.

There is an existing RiRo access located to the north approximately 700 feet from the signalized intersection of SR 141/Peachtree Parkway at East Jones Bridge/Medlock Bridge Road. This would be an ideal location to construct the proposed CFI and median U-turn traffic signal. The existing southbound left turning traffic would be routed to turn left at the new CFI traffic signal.

The existing northbound left turning traffic would be re-routed to make the median U-turn movement at the proposed CFI and median U-turn traffic signal.

The capacity analysis for Build Option (8) can be seen in Tables 20 and 21. Table 20 identifies the LOS and Delay comparisons and Table 21 shows the 95<sup>th</sup> percentile queuing for each movement at each intersection.

Figure 17 – Build Option (8): East Jones Bridge Road Operational Improvements



*Table 20 – LOS and Delay Capacity Analysis: Build Option (8)*

INT #	INTERSECTION	AM PEAK HOUR		PM PEAK HOUR	
		(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
9	SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	F (120.3)	F (97.8)	F (86.5)	D (54.1)
10	SR 141 @ RIRO #15	--	B (15.7)	--	B (18.1)
	Total	120.3	113.5	86.5	72.2

*Table 21 – Queuing Analysis: Build Option (8)*

INT #	INTERSECTION	Movement	AM PEAK HOUR		PM PEAK HOUR	
			(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021	(3rd lane only) Build 2021	(3rd lane with Additional Improvement) Build 2021
9	SR 141 @ East Jones Bridge Rd/ Medlock Bridge Rd	NB	272 ft	168 ft	m410 ft	m96 ft
		SB	#1208 ft	#1100 ft	395 ft	302 ft

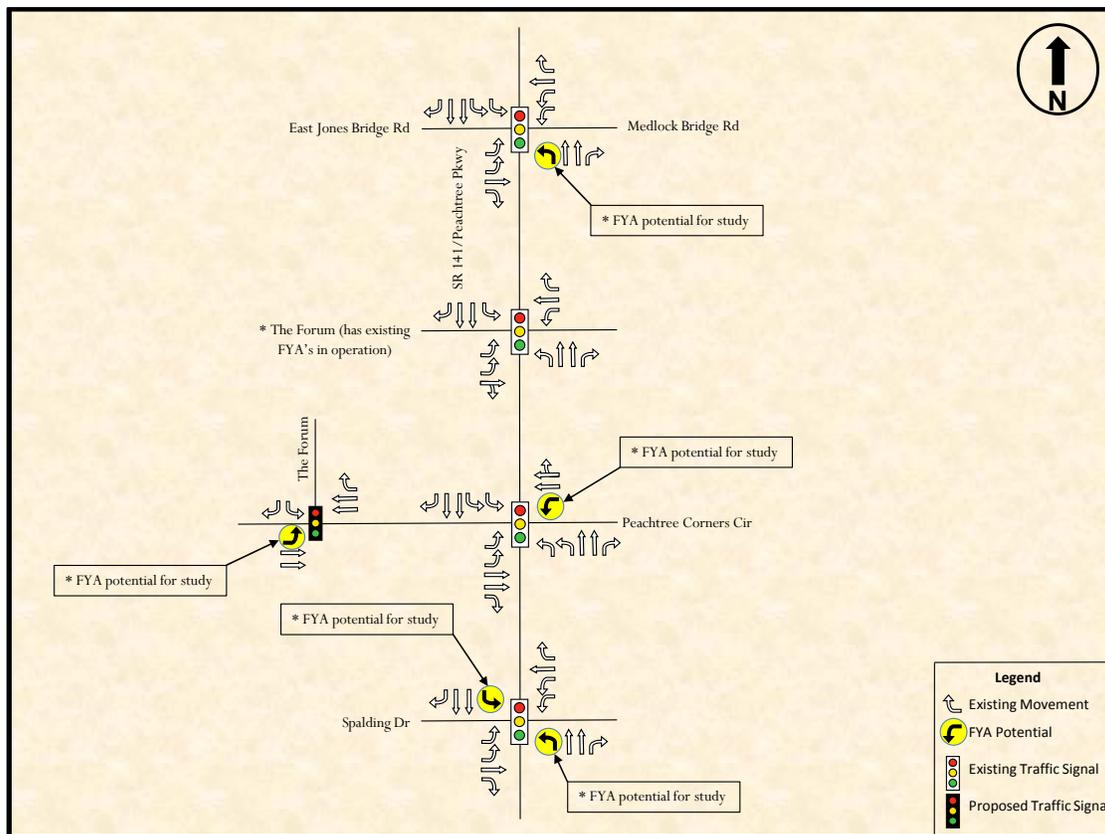
m - volume for queue is metered by upstream signal  
# - volume exceeds capacity, queue may be longer

**Build Option (9) – FYA Implementation at Eligible Signalized Intersections**

Build Option (9) identifies operational improvements at the four (4) study intersections, as well as any proposed signalized intersections, for FYA implementation. FYA’s are being deployed state-wide and can provide operational and safety improvements. The operational benefit for implementing FYA’s can be to add lead/lag signal operations to more efficiently time the traffic signals. Additionally, operational benefits can be realized by removing protected-only, single lane left turns and replacing with FYA’s only if crash and sight distance requirements are met. The safety benefit of implementing FYA’s in place of existing 5-section signal heads is to eliminate the “yellow-trap” safety concern.

The potential locations along this corridor for FYA implementation do not include a safety benefit. There are no existing 5-section signal heads at the four (4) study intersections. All left turn movements at SR 141/Peachtree Parkway at Spalding Drive, Peachtree Corners Circle, The Forum and East Jones Bridge are either existing protected-only or exiting FYA signal head operations. Out of those locations, on the single-lane approach left turn movements are eligible for study since dual left turns are not eligible for FYA implementation. However, out of the protected-only, single-lane approaches, a study would have to be conducted to identify any safety/crash concerns to research why that particular movement is protected-only in the existing conditions. There could be a potential safety, sight distance, or crash concern that supports the single lane movement to remain protected-only. Figure 18 shows the potential locations where FYA implementations could be considered pending a further study.

**Figure 18 – Build Option (9): FYA Potential Locations**



## 5. CONCLUSIONS

---

### SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

Based on the analysis documented in this report, Wolverton and Associates, Inc. make the following conclusions and recommendations.

**(Build Options 1-3).** Based on analyzing Build Options 1-3, it is not recommended to implement median U-turn traffic signals along SR 141/Peachtree Parkway unless a 3<sup>rd</sup> through lane is constructed both northbound and southbound. Additionally, it is also recommended that if any of the median U-turn options are considered, other operational improvements highlighted in Build Options 4-8 should be considered also.

**(Build Option 4).** Regardless of what type of operational improvement is considered along the SR 141/Peachtree Parkway corridor, it is recommended an additional northbound and southbound through lane be constructed. As seen in Tables 13 and 14, simply adding a 3<sup>rd</sup> through lane in each direction along SR 141/Peachtree Parkway dramatically improves the overall performance of the mainline during the peak hours.

**(Build Option 5).** When considering operational improvements at the intersection of SR 141/Peachtree Parkway at Spalding Drive, it is recommended to add an additional through lane eastbound and westbound along Spalding Drive and consider implementing mainline northbound and southbound median U-turn traffic signals. Tables 15 and 16 show significant improvement to this intersection by implementing these operational changes.

**(Build Option 6).** The operational changes analyzed at SR 141/Peachtree Parkway at Peachtree Corners Circle, which include re-routing the eastbound and westbound left turns as well as adding a new traffic signal at The Forum driveway only showed marginal improvements, as seen in Tables 17 and 18. It is not recommended to re-route the eastbound and westbound left turns due to volume to capacity concerns along Peachtree Corners Circle. Also, given the analysis in Build Option (1) does not show significant improvement by adding mainline northbound and southbound median U-turn traffic signals that improvement is not recommended. However, adding a new traffic signal at The Forum and controlling the access along Peachtree Corner Circle could improve both safety and operation during the peak periods.

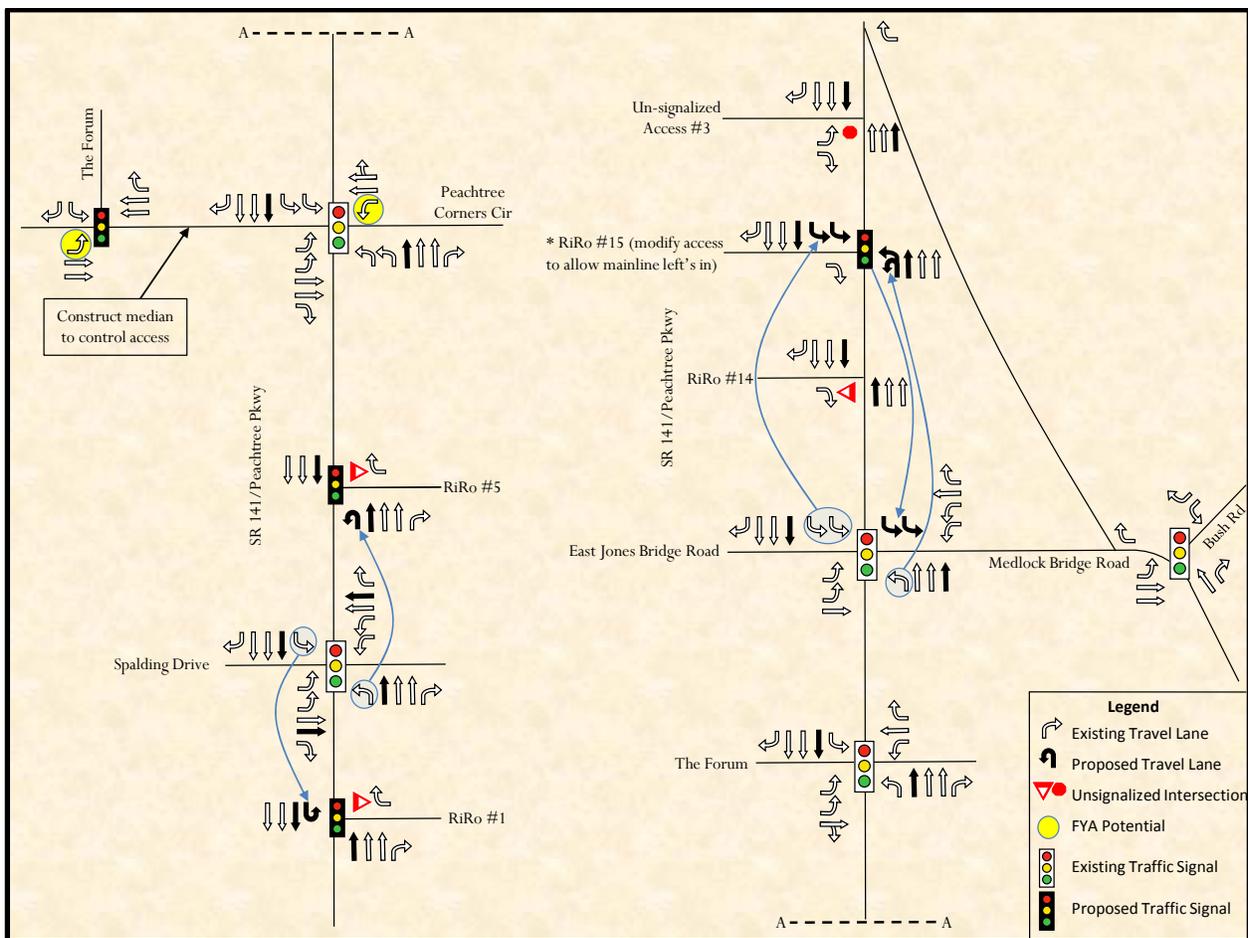
**(Build Option 7).** The operational improvement analyzed at SR 141/Peachtree Parkway at The Forum for removing the full access traffic signal and better spacing the median U-turn signals shows significant improvements compared to just adding a 3<sup>rd</sup> through lane along the mainline, as seen in Table 19. Although this operational changes shows significant improvements, it is not recommended to implement this operational change. It is not known what future developments on the east leg of this intersection will be and thus it is not known how much future traffic will be generated. Given this lack of future development and traffic data, it is not recommended to remove the full access traffic signal.

**(Build Option 8).** The operational improvement analyzed at SR 141/Peachtree Parkway at East Jones Bridge/Medlock Bridge Road for implementing a CFI for the mainline southbound left turn and median U-turn traffic signal for the mainline northbound left turn shows significant improvement during the peak hours, as seen in Tables 20 and 21. It is recommended this operational improvement be considered to be implemented.

**(Build Option 9).** The operational improvement analyzed at SR 141/Peachtree Parkway at Spalding Drive, Peachtree Corners Circle, The Forum East Jones Bridge/Medlock Bridge and any proposed traffic signals identifies potential locations for implementing FYA's. It is recommended to further study any eligible location shown in Figure 18 or 19 for implementing FYA's to gain a better understanding if the eligible locations meet requirements.

The recommendations made are a combination of various Build Options analyzed. It is recommended to implement the improvements outlined in Build Options 5, 6 (only the new traffic signal and median control access), 7 and 8. Each of these have specific improvements at the four (4) study intersections as well as add a 3<sup>rd</sup> northbound and southbound through lane along SR 141/Peachtree Parkway. Figure 19 shows Build Options 5-8 with the recommended improvements identified.

*Figure 19 – Recommended Operational Improvements*



The conclusions drawn in this report are done at the concept level. If one or more of the recommendations are considered to be implemented, an additional GDOT Corridor Study will have to be completed given this corridor resides along a State Route and GDOT has minimum requirements to satisfy the completion of a Corridor Study.

## REFERENCES

---

### SR 141 MEDIAN U-TURN TRAFFIC ENGINEERING REPORT

1. Synchro, Version 9, Trafficware Ltd., Sugar Land, TX, 2015.
2. Highway Capacity Manual, HCM 2000, Transportation Research Board, Washington, DC, 2000.
3. Manual on Uniform Traffic Control Devices, 2009 Edition, Federal Highway Administration, Washington, DC, 2009.

**Work Session**

**Banners**

**G. Ramsey**

Bin 10140  
241 Ralph McGill Boulevard NE  
Atlanta, GA 30308-3374  
404-506-2928



March 15, 2016

City of Peachtree Corners  
Attn: Gregory Ramsey  
147 Technology Pkwy  
Suite 200  
Peachtree Corners, GA 30092

RE: Pole Attachment Agreement

Dear Mr. Ramsey:

Attached is the proposed pole attachment agreement between City of Peachtree Corners and Georgia Power Company.

This template agreement has been discussed, submitted, and verbally accepted by GMA and has been fully accepted by ACCG over the course of several months. The main obstacles to agreement had been the requirements for providing insurance coverage. However the document attached, including the insurance language has been signed off on for allowing cities and counties to attach certain attachments to Georgia Power Company poles. We decided on the course of action taken so that we would not have to negotiate individually with all cities and counties on the terms and conditions of the agreement.

In addition to the agreement we have developed two addendums for special equipment. One is for banners and seasonal decorations. The other is for wireless attachments which quite often require special considerations. Installation of wireless camera installations and Wi-Fi devices would require the execution of the wireless addendum. If the City has any thought of attaching wireless devices, seasonal decorations or banners in the future, please consider signing the addendums to prevent delays in installation.

For all three of the attached documents the following information will be required:

We will need three (3) originals of each signed and returned to me. After we have signed them, I will send the City a fully executed agreement for its records.

For the agreement please ensure that:

1. The contractual name is correct on Pages 1, 10, and 11
2. Notices are directed to the correct location and person on page 8
3. The signature page is completed on page 10
4. The signature page is completed on Page 11 **only if the City wants to be a party to a transfer agreement.** (Please consider this option. We believe it will benefit all parties.)
5. All information requested on Exhibit C on page 14 is provided

For both the banner addendum and the wireless addendum, initially there is no information required other than the proper signatures and approval. The Exhibit A, B, and C information for each of these will be required at the time of application to attach to specific poles.

If I can answer any questions or if you wish to contact me, please feel free to do so by telephone at (404) 506-2928 or by email at [jdwilson@southernco.com](mailto:jdwilson@southernco.com)

Sincerely,



J. Darryll Wilson

Bin 10140  
241 Ralph McGill Boulevard NE  
Atlanta, GA 30308-3374  
404-506-2928  
**Joint Use Coordinator**

**Georgia Power Company  
Governmental Pole  
Attachment  
License Agreement**

Between  
Georgia Power Company  
And  
City of Peachtree Corners

## TABLE OF CONTENTS

	<b>Page</b>
1. DEFINITIONS.....	1
2. TERM .....	2
3. LICENSE .....	2
4. RESTRICTIONS ON USE.....	3
5. RIGHTS-OF-WAY AND EASEMENTS.....	4
6. OVERLASHING .....	4
7. MAKE-READY WORK .....	4
8. SAFETY COMPLIANCE .....	5
9. TRANSFER AND REARRANGMENT .....	6
10. POLE ATTACHMENT COUNTS .....	6
11. PAST DUE PAYMENTS .....	6
12. INSURANCE.....	6
13. RELEASE AND LIMITATION OF LIABILITY .....	6
14. WARRANTY EXCLUSIONS.....	7
15. REPAIR AND OTHER WORK PERFORMED BY GPC.....	7
16. NOTICE.....	7
17. MARKING OF OVERHEAD CABLES .....	8
18. LICENSEE INFORMATION.....	8
19. FORCE MAJEURE .....	8
20. MISCELLANEOUS .....	8
21. EXHIBIT A.....	11
22. EXHIBIT B.....	12
23. EXHIBIT C.....	14

**GEORGIA POWER COMPANY POLE ATTACHMENT LICENSE  
AGREEMENT - GOVERNMENT USE**

THIS POLE ATTACHMENT LICENSE AGREEMENT (“Agreement”) is made between **GEORGIA POWER COMPANY** (hereinafter “GPC”) and **CITY OF PEACHTREE CORNERS**, a municipality of the State of Georgia (hereinafter “Licensee”), and is subject to the following terms and conditions:

**1. DEFINITIONS:**

**A.** “Attachments” means all cables, wires, power supply equipment and cameras and all associated hardware and equipment reasonably necessary for the attachment of said cables, wires, power supply equipment and cameras, owned and/or utilized by Licensee that now or in the future occupy any Pole, including any Overlashing, and shall not include access to any duct, conduit or right-of-way owned by GPC, which may in GPC’s sole discretion be made available under a separate Master Conduit License Agreement or other appropriate contract.

**B.** “Effective Date” means the date on which GPC makes written acceptance of Licensee’s signed copy of this Agreement.

**C.** “FCC” means Federal Communications Commission.

**D.** “ILEC” means an incumbent local exchange carrier as defined in 47 U.S.C. § 251(h).

**E.** “Inherent Dangers” means the transmission/distribution of electrical energy involves the handling of a natural force that, when uncontrolled, is inherently hazardous to life and property. Due to the nature of any work to be performed, other hazardous or dangerous conditions (not necessarily related to the inherent danger of electricity) may also be involved in performing work on Poles, including but not limited to the following dangers: physically tripping, falling objects from work being performed overhead, working on scaffolding, electric wiring, hot metal surfaces, flash burns from welding, use of heavy equipment (e.g., mobile and stationary cranes and hoists, trucks and other equipment normally used in heavy construction and hauling), and possible hazardous materials, substances and chemicals, including the presence of asbestos, polychlorinated biphenyl (PCB), solvents, lead paint and arsenic.

**F.** “Inspection” means any inspection to determine if an Attachment or work being performed by Licensee or its contractors thereon is in compliance with Legal Requirements and the terms of this Agreement.

**G.** “Joint User” means any public utility, governmental body or other entity that owns poles to which GPC has extended or may hereafter extend joint use privileges whereby GPC and such party may affix their equipment to each other’s poles.

**H.** “Legal Requirements” means all applicable statutes, laws, rules, codes, ordinances, regulations, decisions, orders, decrees, policies, guidance, directives or common law of any Federal, state, local or other governmental body, authority or entity with jurisdiction over the subject matter of this Agreement, contract performance or the applicable party (including any judicial or administrative interpretation), that: (1) are: (a) in effect on the Effective Date; or (b)

imposed during the term of this Agreement; and (2) which in any manner affect the subject matter of this Agreement, contract performance or this Agreement, including without limitation the NESC and any other applicable safety codes and regulations.

I. “Make-Ready Project Manager” means Candler Ginn or his successor.

J. “Make-Ready Work” means all work, as reasonably determined by GPC, required to accommodate an Attachment and to comply with Legal Requirements, including but not limited to rearrangements, increasing the load bearing ability of a Pole and/or the transfer or replacement of a Pole and other reasonable requirements of GPC. Make-Ready Work also includes but is not limited to Inspections, engineering, permitting and construction.

K. “NESC” means the National Electrical Safety Code.

L. “NJUNS” means the National Joint Utilities Notification System.

M. “Overlashing” means the tying, draping, twisting, wrapping or otherwise attaching of fiber optic cable, coaxial cable or other wires over or around other host cables or wires attached to a Pole.

N. “Point of Attachment” means the approved point of attachment for an Attachment, as designated by GPC.

O. “Pole” means a GPC distribution pole.

P. “Pole Attachment Count” means a count of Licensee’s Attachments.

Q. “PSC” means the Georgia Public Service Commission.

R. “RF” means Radio Frequency.

S. “Transmission Facility” means any of GPC’s electric supply lines and support structures operated at or above 69 kilovolts (kV).

2. **TERM:** This Agreement shall be effective as of the Effective Date and shall remain in full force and effect until terminated in accordance with the provisions of this Agreement. Either party may terminate this Agreement upon one hundred eighty (180) days’ prior written notice. Each license granted hereunder shall continue in effect until the first to occur of the following: (i) Licensee notifies GPC in writing that it is removing the Attachment allowed under such license; (ii) GPC terminates such license pursuant to the terms of this Agreement; or (iii) this Agreement terminates.

### 3. **LICENSE:**

A. **Application.** Prior to affixing any Attachment, Licensee shall request approval on a form to be provided upon request by the Make-Ready Project Manager. Licensee shall submit reasonably sufficient data (including without limitation sag, tension, design and loading data) for such Attachment, in form prescribed by GPC, such that GPC can ascertain whether Licensee’s

construction plan complies with the requirements set forth in such data and applicable industry standards.

**B. Permission.** GPC shall approve or deny the application, in its sole discretion, within forty-five (45) days of receipt thereof, and may do so via electronic means, including NJUNS. After approval of an Attachment, GPC will perform Make-Ready Work and issue a conditional permit in accordance with Section 7 hereinbelow. Licensee shall not affix an Attachment until receipt of said permit. Notwithstanding the foregoing, each Attachment in place on the Effective Date that has been approved in writing by GPC is hereby granted a separate license to be governed by this Agreement.

**C. Attachment Rate.** To the extent permitted by applicable statutes and regulations, GPC hereby waives the right to collect a rental fee for any Attachment approved by GPC pursuant to this Agreement, provided, however, that Licensee shall not change the type or use of such Attachment as approved.

**D. Service Drops.** Licensee shall not place a service drop unless and until it has received prior written permission from GPC.

**E. Removal.**

**i.** Licensee must promptly notify GPC in writing when Licensee removes an Attachment. Licensee will continue to be responsible for any rental fees for removed Attachments until GPC receives written notice of removal.

**ii.** Upon notice from GPC that it requires use of any Attachment space (which notice shall include an estimate of the costs GPC would incur in expanding capacity necessary to accommodate both the Attachment and GPC's core utility service), Licensee shall, within thirty (30) days of receiving such notice, either remove the Attachment or pay GPC's cost of expanding capacity necessary to accommodate both the Attachment and GPC's core utility service. If Licensee shall fail to do so, GPC shall be permitted to, at its option, either remove the Attachment, or increase capacity to accommodate GPC's core utility service, and Licensee shall reimburse GPC for all reasonable costs incurred for same.

**iii.** Upon notice from GPC that it is abandoning a Pole, Licensee shall, within thirty (30) days of receiving such notice, remove its Attachments from said Pole. In the event that Licensee fails to do so, GPC shall have the right to remove or cause to be removed any such Attachments and Licensee shall reimburse GPC for all reasonable costs incurred for same.

**iv.** Licensee shall remove an Attachment no later than thirty (30) days following termination of the applicable license and/or this Agreement. Notwithstanding anything herein to the contrary, Licensee shall remove any unauthorized or unapproved Attachment promptly upon notice from GPC.

**4. RESTRICTIONS ON USE:**

**A. Purpose.** Licensee is a governmental entity and will affix Attachments for the sole purpose of internally conducting government business, which shall not include providing

services for a profit. Licensee shall not affix Attachments for the purpose of providing wireless internet service to the public, nor shall Licensee affix any Attachments that would be subject to the mandatory access requirements of 47 U.S.C. § 224(f).

**B. Change in Type or Use.** Licensee will not make any change to the type or use of its Attachments without prior written approval from GPC.

**C. Cameras.** Licensee shall limit the use of camera Attachments to lawful crime prevention and investigation. When required by a Legal Requirement, Licensee shall obtain any necessary authority or order from a court with appropriate jurisdiction and provide GPC with a copy of same prior to Licensee's use of any camera.

**D. Wireless.** Licensee shall not affix any wireless Attachments unless and until it has entered into a separate wireless addendum to this Agreement, the form of which the Make-Ready Project Manager will provide upon request by Licensee; and Licensee shall only affix wireless Attachments in accordance with said addendum.

**5. RIGHTS-OF-WAY AND EASEMENTS:** GPC does not warrant the extent of its rights-of-way or easements. Licensee shall be responsible for obtaining any real property rights necessary for Attachments on a Pole. If GPC determines that Licensee's use of a Pole is not permitted or is prohibited by the underlying property owner, Licensee shall, upon notice from GPC, promptly remove its Attachments from such Pole.

**6. OVERLASHING:** Licensee must obtain written approval from GPC prior to Overlashing. Each request for approval must state the intended use of the Overlashing and must include a certification by Licensee that the Overlashing will be in compliance with the NESC and include reasonably sufficient data (including without limitation sag, tension, design and loading data) for such Overlashing, in form prescribed by GPC, such that GPC can ascertain whether Licensee's construction plan complies with the requirements set forth in such data and applicable industry standards.

**7. MAKE-READY WORK:**

**A. Who May Perform.** Only GPC and its contractors are authorized to perform Make-Ready Work, provided, however, that Licensee shall be permitted to rearrange its own existing Attachments. Licensee shall never work on or impact another party's attachments, except where such party has given Licensee written approval to perform such work. All Make-Ready Work shall be performed in accordance with all Legal Requirements.

**B. Payment.** For an approved Attachment, GPC shall provide Licensee with an engineering survey and the estimated cost of Make-Ready Work within fourteen (14) days of conditional approval of the Licensee's permit. The engineering survey will depict the Point of Attachment. The estimate will remain valid for fourteen (14) days following notification. Upon receipt of Licensee's written acceptance of the estimated cost, GPC will invoice Licensee for the estimated cost. Make-Ready Work will not begin until GPC receives full payment of the estimated cost, which Licensee shall pay to GPC within thirty (30) days of the invoice date. If the actual cost exceeds the estimated cost, GPC will invoice Licensee for the excess cost and Licensee shall pay the excess costs to GPC within thirty (30) days of the invoice date. If the

actual cost is less than the amount tendered, GPC will refund the excess payment amount. The total cost for Make-Ready Work shall include any and all costs incurred by GPC in connection with the Make-Ready Work.

**C. Location.** Attachments must be placed at the Point of Attachment and must not interfere with any other party's equipment on the Pole. The Attachment space for wire Attachments is six (6) inches above and below the Point of Attachment. The Attachment space for wireless Attachments will be determined by GPC by the space occupied by such Attachments. Licensee shall not use a GPC anchor or affix or attempt to affix an Attachment to a Transmission Facility without the express written approval of GPC.

**D. Affixing.** Following completion of Make-Ready Work, GPC will issue a conditional permit to Licensee to affix its Attachment. The Attachment must be affixed and any related construction work completed within one hundred twenty (120) days of permit issuance. Licensee will notify GPC in writing at least three (3) business days prior to starting work on the Attachment and within three (3) business days of completing same. GPC or its contractors will perform an Inspection. In the event that GPC determines that an Attachment or any work thereon fails to comply with any Legal Requirements or the terms of this Agreement, GPC will so notify Licensee and Licensee shall make any necessary repairs or corrections within thirty (30) days thereafter, or immediately if the violation creates a danger to persons or property. GPC or its contractors will then perform further Inspections as necessary until it determines that Licensee is in compliance with all Legal Requirements and the terms of this Agreement.

## **8. SAFETY COMPLIANCE:**

**A.** Licensee shall only use persons to perform work on Poles who are qualified by the education and experience necessary to provide high quality performance of work on Poles and who possess each license, registration, certification or other qualification required by any Legal Requirements to perform any work contemplated by this Agreement to be performed by Licensee or its contractors. Such persons must exercise that degree of skill and care required by the highest level of accepted professional standards with respect to performing work on Poles and shall have been properly trained on the hazards as set forth in 19 C.F.R. § 1910, Subpart S and § 1910.269. Licensee is solely responsible for assuring such persons have the necessary qualifications, expertise and safety training.

**B.** Before any person has performed any work contemplated by this Agreement by, through or for Licensee on or near any facilities of GPC, Licensee must adequately instruct and warn such person concerning the hazards and Inherent Dangers associated with performing work on Poles, including without limitation the danger inherent in making contact with GPC's electrical conductors and of coming closer to such conductors than is permitted by the NESC or by regulations of the Occupational Safety and Health Administration.

**C.** Licensee shall ensure that all Attachments and any equipment and work in connection therewith are in compliance with all Legal Requirements, including but not limited to separation and clearance requirements. Licensee shall use its best efforts to correct any safety violations that are the responsibility of Licensee within thirty (30) days of Licensee having

knowledge of same, except for violations creating a danger to persons or property, which must be corrected immediately upon discovery.

**9. TRANSFER AND REARRANGMENT:**

**A.** Licensee may authorize GPC to transfer its Attachments when necessary due to a relocation or replacement of a Pole, at Licensee's expense and in accordance with the terms and conditions set forth on **Exhibit A** attached hereto, incorporated herein by reference and made a part hereof, by executing **Exhibit A**.

**B.** In the event that an Attachment must be rearranged or transferred in order to accommodate another party requesting to attach, GPC, its contractors or the party requesting to attach shall be permitted to rearrange or transfer Licensee's Attachment if Licensee has failed to do so within thirty (30) days of notice from Licensor, or such other time period as may be required by applicable Legal Requirements.

**10. POLE ATTACHMENT COUNTS:** GPC shall not charge Licensee for any Pole Attachment Count for Attachments that are both (a) validly permitted under and in compliance with this Agreement and (b) not subject to the mandatory access requirements of 47 U.S.C. § 224(f).

**11. PAST DUE PAYMENTS:** In addition to any other right or remedy available to GPC, if Licensee fails to make any payment under this Agreement within thirty (30) days after it becomes due and payable, then Licensee will pay interest thereon at a rate equal to the lesser of one and one-half percent (1.5%) per month or the highest rate permitted by law. GPC will apply all payments to interest first and then to principal in the order in which it has become due. To the extent allowed by Georgia law, Licensee will reimburse GPC for all expenses that GPC incurs in connection with collection of any past due payment, including without limitation legal fees.

**12. INSURANCE:** Licensee shall procure and maintain insurance to protect it and GPC against claims for damage to property and bodily injury or the death of persons in the amount of \$1,000,000 for damages arising from one occurrence and \$1,000,000 for aggregate claims. Licensee shall also carry insurance sufficient to cover claims under workmen's compensation laws for Licensee's operations within GPC's service area. All such insurance shall be kept in force by Licensee for the life of this Agreement and the company or companies issuing such insurance shall be subject to the reasonable approval of GPC. GPC shall be named an additional insured under Licensee's liability insurance policy and the policy must state that the insurance company is waiving any right of subrogation against GPC. Licensee shall furnish to GPC a certificate showing the issuance of such insurance and the insurance company's agreement that it will not cancel, terminate, or change its policy except after thirty (30) days' prior written notice to GPC. Licensee's failure to maintain insurance coverage as required by this Agreement shall be deemed a material breach and Licensee's rights to affix Attachments shall be suspended until Licensee is in full compliance with the foregoing insurance requirements.

**13. RELEASE AND LIMITATION OF LIABILITY:** TO THE EXTENT ALLOWED BY GEORGIA LAW, LICENSEE ACKNOWLEDGES AND AGREES THAT GPC'S

LIABILITY FOR DAMAGES TO PROPERTY, BODILY INJURY OR DEATH TO ANY PERSON SHALL BE LIMITED SOLELY TO ACTS AND OMISSIONS CAUSED BY GPC'S NEGLIGENCE, AND HEREBY AGREES TO AND RELEASES GPC FROM ANY AND ALL LIABILITY FOR DAMAGES TO PROPERTY, BODILY INJURY OR DEATH TO ANY PERSON EXCEPT TO THE EXTENT CAUSED BY GPC'S NEGLIGENCE. LICENSEE FURTHER AGREES THAT, TO THE EXTENT ALLOWED BY GEORGIA LAW, GPC SHALL NOT BE LIABLE FOR INDIRECT, INCIDENTAL OR CONSEQUENTIAL DAMAGES OR FOR ANY LOST PROFITS, SAVINGS OR REVENUES OF ANY KIND AND THAT ANY CLAIM BROUGHT AGAINST GPC BY A THIRD PARTY ARISING FROM AN ACT OR OMISSION OF LICENSEE SHALL BE SUBJECT TO AND COVERED BY THE INSURANCE POLICY LICENSEE ACQUIRES PURSUANT TO ITS CONTRACTUAL OBLIGATIONS UNDER THIS AGREEMENT, PROVIDED THAT ANY SUCH POLICY SHALL PROVIDE COVERAGE IN ACCORDANCE WITH, AND NOT LESS THAN, THAT DESCRIBED IN THE TERMS AND CONDITIONS OF THE ACCGIRMA COVERAGE AGREEMENT EFFECTIVE 1 JULY 2011, AS ENDORSED TO ADD GPC AS AN "ADDITIONAL MEMBER."

**14. WARRANTY EXCLUSIONS:** GPC, ITS AFFILIATES, CONTRACTORS, SUBCONTRACTORS AND SUPPLIERS MAKE NO WARRANTIES OR REPRESENTATIONS OF ANY KIND OR CHARACTER, EXPRESS, IMPLIED, STATUTORY OR OTHERWISE AND SPECIFICALLY DISCLAIM ANY WARRANTY OF MERCHANTABILITY OR WARRANTY OF FITNESS FOR A PARTICULAR PURPOSE REGARDING THE CONDITION OF ANY POLE OR ANY SERVICE MADE AVAILABLE TO LICENSEE UNDER THIS AGREEMENT OR ANY WORK PERFORMED BY GPC, ITS AFFILIATES, CONTRACTORS, SUBCONTRACTORS AND SUPPLIERS. NO APPROVAL TO AFFIX AN ATTACHMENT SHALL CONSTITUTE A GUARANTEE OR REPRESENTATION THAT ADEQUATE SPACE EXISTS FOR SUCH ATTACHMENT ON ANY POLE AT THAT TIME OR IN THE FUTURE.

**15. REPAIR AND OTHER WORK PERFORMED BY GPC:** In the event that GPC makes repairs or performs any work, other than transfers and rearrangements pursuant to Section 9(A) and Exhibit A to this agreement, to a Pole on which Licensee has an Attachment, GPC will bill Licensee for the actual costs of any portion of said repairs or other work that directly benefit Licensee, as determined by GPC, and Licensee shall make payment to GPC within thirty (30) days of the date of invoice. In the event GPC determines that any work or repairs (including but not limited to removal of Attachments) are necessary because of a violation by Licensee of this Agreement or because of an attachment by Licensee that is not in compliance with this Agreement, GPC or its contractors shall be entitled to perform such work or repairs. Licensee shall be responsible for the full cost of such work or repairs and shall make payment to GPC within thirty (30) days of the date of invoice for same.

**16. NOTICE:** All notices regarding the affixing, maintenance or removal of an Attachment shall be sent electronically using NJUNS. All other notices shall be sent by electronic mail, facsimile or in written form delivered either personally or by mail, courier or similar reliable means of dispatch to the following address:

GPC:

Georgia Power Company  
Attn: Joint Use Coordinator  
241 Ralph McGill Boulevard, N. E.  
Bin 10140  
Atlanta, GA 30308-3374  
Email: jdwilson@southernco.com  
Facsimile: 404-506-2925

**For permit applications:**

Georgia Power Company  
Attn: Make-Ready Project Manager  
829 Jefferson Street  
Atlanta, GA 30318

Licensee:

City of Peachtree Corners  
Attn: George Ramsey, Director of Public Works  
147 Technology Pkwy  
Suite 200  
Peachtree Corners, GA 30092

**17. MARKING OF OVERHEAD CABLES:** Licensee shall mark all of its overhead cables attached to Poles in accordance with the marking standards set forth in **Exhibit B** attached hereto, incorporated herein by reference and made a part hereof. In the event that Licensee fails to mark an overhead cable, GPC shall have the right, at Licensee's expense, to mark same.

**18. LICENSEE INFORMATION:** Licensee must provide the information requested on **Exhibit C** attached hereto, incorporated herein by reference and made a part hereof, on or before the Effective Date. Licensee shall provide GPC with written notice within thirty (30) days of any change to such information.

**19. FORCE MAJEURE:** Neither GPC nor Licensee shall be liable for any delay, failure in performance, loss or damage due to force majeure conditions, including but not limited to fire, explosion, power failures, pest damage, lightning, extreme heat or cold, power surges, strikes, labor disputes, water, acts of God, the elements, war, civil disturbance, acts of civil or military authorities or the public enemy, inability to secure raw materials including Poles, inability to obtain transportation, fuel or energy shortages, failure of either performance or availability of communication services or network poles, failure of an ILEC or third-party utility to act notwithstanding reasonable efforts on the part of GPC or the Licensee or other causes beyond the parties' control.

**20. MISCELLANEOUS:** Licensee shall comply, and shall require that its contractors comply, at all times with all Legal Requirements. Licensee may not assign this Agreement in

whole or in part without the prior written consent of GPC and shall not permit any third party to overlash or affix any attachments. This Agreement shall be governed by and construed in accordance with the laws of the State of Georgia. Any litigation arising from any alleged breach of this Agreement must be brought in an appropriate court in Fulton County, Georgia. Any headings inserted herein are for convenience only, and shall not add to or subtract from the meaning of the contents of any provision herein. This Agreement may only be modified by a writing executed by both parties. Failure by GPC to enforce any term of this Agreement shall not constitute a waiver of future compliance with any such term. If any provision of this Agreement is found to be illegal or otherwise invalid, the validity of the remaining provisions shall not be impaired. The parties shall attempt to replace any invalid provision with a valid provision having substantially the same commercial effect as such invalid provision and the replacement provision shall be deemed effective retroactively to the Effective Date. This Agreement may be executed in multiple counterparts, each of which shall constitute an original, but all of which taken together shall constitute one and the same agreement. In the event this Agreement is terminated, the terms and conditions set forth herein shall survive and continue in force with respect to: (i) determining either party's failure to perform during the term of the Agreement; (ii) any failure by Licensee to move or remove its Attachments or to comply with applicable safety standards; and (iii) determining Licensee's responsibility regarding Licensee's assumption of ownership of abandoned Poles. This is the entire Agreement between the parties with respect to Attachment rights and supersedes all prior agreements, proposals, communications and understandings between the parties concerning the subject matter contained herein.

**[SIGNATURES COMMENCE ON NEXT PAGE]**

**LICENSEE:**

Adopted at \_\_\_\_\_, Georgia this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.

**CITY OF PEACHTREE CORNERS**

By: \_\_\_\_\_

Mayor  
City of Peachtree Corners

Attest:

\_\_\_\_\_  
Name & Title

Entered upon the Minutes, Minute Book \_\_\_\_\_, Page \_\_\_\_\_.

Date: \_\_\_\_\_

**GPC:**

**GEORGIA POWER COMPANY**

By: \_\_\_\_\_ (SEAL)

Leslie R. Sibert, Distribution Vice President

Date: \_\_\_\_\_

**EXHIBIT A**

**TRANSFER OF EQUIPMENT**

**RECITALS**

Pursuant and subject to the foregoing Agreement, Licensee holds a license to affix Attachments. Licensee wants GPC to transfer its Attachments when GPC transfers its own electric distribution facilities during the replacement or relocation of Poles. GPC is willing to transfer such Attachments on the terms and conditions set forth in this **Exhibit A**. Accordingly, for value received, the parties agree as follows:

**TERMS AND CONDITIONS**

1. **TRANSFER OF ATTACHMENTS.** If GPC is relocating or replacing a Pole and determines that the transfer of Licensee’s Attachments on such Pole is appropriate, then GPC or its contractors may transfer such Attachments concurrent with the transfer of GPC’s own equipment attached to such Pole. If GPC replaces or relocates a Pole, but does not transfer Licensee’s Attachments, GPC will notify Licensee so as to enable Licensee to make such transfer, and the Agreement will govern the rights and obligations of the parties with respect to such Pole.
2. **FEES.** Licensee will pay GPC, within thirty (30) days of the date of invoice, GPC’s standard fee for any work performed by GPC or its contractors under this **Exhibit A** as established and amended by GPC from time to time. As of the date indicated below, the standard fee is \$85.00 per Attachment.
3. **GOVERNING AGREEMENT.** In performing their obligations under this **Exhibit A**, the parties will remain governed by the terms and conditions of the Agreement, including without limitation those provisions relating to limitation of liability.
4. **TERMINATION.** This **Exhibit A** may be terminated by either party upon written notice to the other party.

Licensee hereby accepts and authorizes GPC or its contractors to transfer its Attachments in accordance with the foregoing terms and conditions.

**LICENSEE:**

**GPC:**

**CITY OF PEACHTREE CORNERS**

**GEORGIA POWER COMPANY**

By: \_\_\_\_\_  
Name: \_\_\_\_\_  
Title: \_\_\_\_\_  
Date: \_\_\_\_\_

By: \_\_\_\_\_  
Leslie R. Sibert  
Distribution Vice President  
Date: \_\_\_\_\_

## **EXHIBIT B**

### **GPC OVERHEAD CABLE MARKING STANDARD**

Licensee shall mark all of its Attachments in accordance with the following standards:

#### **NJUNS - CTY - OPTIONAL**

**NJUNS = NJUNS\* Member Code (up to 6 alphanumeric digits)**  
**CTY = County Code from County Rate Chart (3 digit number)**  
**OPTIONAL = Optional space for attachee's internal use (Any length)**

Example: **OWNER1 - 033 - Marietta**

NJUNS member code for owner of Attachment is "OWNER1"  
County where Attachment is located is Cobb County.  
Franchised area where Attachment is located is Marietta.

Requirements:

On installation, Attachments shall be marked on the following Poles:

- Every end pole.
- Every junction pole.
- Every 5th pole.

Multiple Attachments on a Pole must each be marked.

On older lines not marked, Attachments should be marked as Poles are visited.

If Licensee's name changes or if Licensee acquires or sells Attachments, Licensee shall revise the NJUNS website table to reflect the new information as soon as the website table is available.

The marker shall have a life span of at least ten (10) years.

Letter height shall be at least three-quarters of one inch (3/4") tall.

Letters shall be black on an orange background.

The marker shall be legible from the ground.

\*NJUNS = National Joint Utilities Notification System ([www.njuns.com](http://www.njuns.com))

## COUNTY CODE CHART

County Code	County	County Code	County	County Code	County
001	APPLING	054	EVANS	107	NEWTON
002	ATKINSON	055	FANNIN	108	OCONEE
003	BACON	056	FAYETTE	109	OGLETHORPE
004	BAKER	057	FLOYD	110	PAULDING
005	BALDWIN	058	FORSYTH	111	PEACH
006	BANKS	059	FRANKLIN	112	PICKENS
007	BARROW	060	FULTON	113	PIERCE
008	BARTOW	061	GILMER	114	PIKE
009	BEN HILL	062	GLASCOCK	115	POLK
010	BERRIEN	063	GLYNN	116	PULASKI
011	BIBB	064	GORDON	117	PUTNAM
012	BLECKLEY	065	GRADY	118	QUITMAN
013	BRANTLEY	066	GREENE	119	RABUN
014	BROOKS	067	GWINNETT	120	RANDOLPH
015	BRYAN	068	HABERSHAM	121	RICHMOND
016	BULLOCH	069	HALL	122	ROCKDALE
017	BURKE	070	HANCOCK	123	SCHLEY
018	BUTTS	071	HARALSON	124	SCREVEN
019	CALHOUN	072	HARRIS	125	SEMINOLE
020	CAMDEN	073	HART	126	SPALDING
021	CANDLER	074	HEARD	127	STEPHENS
022	CARROLL	075	HENRY	128	STEWART
023	CATOOSA	076	HOUSTON	129	SUMTER
024	CHARLTON	077	IRWIN	130	TALBOT
025	CHATHAM	078	JACKSON	131	TALIAFERRO
026	CHATTAHOOCHEE	079	JASPER	132	TATNALL
027	CHATTOOGA	080	JEFF DAVIS	133	TAYLOR
028	CHEROKEE	081	JEFFERSON	134	TELFAIR
029	CLARKE	082	JENKINS	135	TERRELL
030	CLAY	083	JOHNSON	136	THOMAS
031	CLAYTON	084	JONES	137	TIFT
032	CLINCH	085	LAMAR	138	TOOMBS
033	COBB	086	LANIER	139	TOWNS
034	COFFEE	087	LAURENS	140	TREUTLEN
035	COLQUITT	088	LEE	141	TROUP
036	COLUMBIA	089	LIBERTY	142	TURNER
037	COOK	090	LINCOLN	143	TWIGGS
038	COWETA	091	LONG	144	UNION
039	CRAWFORD	092	LOWNDES	145	UPSON
040	CRISP	093	LUMPKIN	146	WALKER
041	DADE	094	MACON	147	WALTON
042	DAWSON	095	MADISON	148	WARE
043	DECATUR	096	MARION	149	WARREN
044	DEKALB	097	McDUFFIE	150	WASHINGTON
045	DODGE	098	McINTOSH	151	WAYNE
046	DOOLY	099	MERIWETHER	152	WEBSTER
047	DOUGHERTY	100	MILLER	153	WHEELER
048	DOUGLAS	101	MITCHELL	154	WHITE
049	EARLY	102	MONROE	155	WHITFIELD
050	ECHOLS	103	MONTGOMERY	156	WILCOX
051	EFFINGHAM	104	MORGAN	157	WILKES
052	ELBERT	105	MURRAY	158	WILKINSON
053	EMANUEL	106	MUSCOGEE	159	WORTH

**EXHIBIT C**  
**(Provide Information on Additional Pages if Necessary)**

Licensee's Attachments will provide the following services to Licensee's customers:

---

---

---

---

Licensee intends to affix Attachments in the following counties pursuant to this Agreement:

---

---

---

---

(Note: If Attachments will only be in part of a county, provide the names of roads or streets that most closely constitute a boundary of the service areas.)

Licensee's NJUNS Member Code and contact e-mail address are as follows:

---

---

---

---

Licensee's applicable FCC and PSC license numbers for service to be provided by its Attachments are as follows:

FCC License Number(s): \_\_\_\_\_

PSC License Number(s): \_\_\_\_\_

\_\_\_\_\_

**Georgia Power Company  
Governmental Pole  
Attachment  
License Agreement**

Between  
Georgia Power Company  
and  
City of Peachtree Corners

Addendum for  
Attachment of Wireless Facilities

WHEREAS, Georgia Power Company (hereinafter “Georgia Power”) and City of Peachtree Corners (hereinafter “Licensee”) have entered into a Pole Attachment License Agreement (hereinafter “Agreement”), dated \_\_\_\_\_; and

WHEREAS, Georgia Power and Licensee mutually desire to expand the scope of the Agreement to allow for the Attachment of Wireless Facilities to the Poles of Georgia Power;

NOW THEREFORE, in consideration of the mutual promises herein, Georgia Power and Licensee agree as follows:

1. The terms and conditions of the Agreement shall govern the Attachment of Wireless Facilities to the Poles of Georgia Power, except as explicitly stated in this Addendum.
2. “Wireless Facilities” means, without limitation, antennas, feedlines, transmitters, receivers, power supplies and all other equipment used or useful to receive or to generate and emit radio frequency (“rf”) energy, including any “intentional radiator,” as defined in Section 15.3(o) of the FCC’s rules.
3. Licensee may not affix Wireless Facilities to any Pole until Licensee requests and receives written permission from Georgia Power, using the form attached hereto as Exhibit A, “Request for Permission to Attach Wireless Facilities.” Such Request(s) for Permission to Attach Wireless Facilities shall include information as to the weight and dimensions of the Wireless Facilities, proposed location on the Pole of the Wireless Facilities, drawings and schematic diagrams of the structural design of the Wireless Facilities and any additional information reasonably requested by Georgia Power as necessary for Georgia Power to perform a pole loading analysis of the proposed Wireless Facilities. A Request for Permission to Attach Wireless Facilities shall be submitted for each Pole to which attachments are proposed. Licensee shall notify third party entities, who already have attachments on Poles to which Licensee is seeking to attach, of Licensee’s intention to attach Wireless Facilities to the same Poles.
4. Licensee’s Wireless Facilities Attachments to Poles must be located in a space designated by Georgia Power. The Wireless Facilities Attachments must comply with: all NESC requirements, including, but not limited to, separation and clearance requirements; all FCC, OSHA and other state and federal agency requirements; and all Georgia Power construction standards and requirements. Licensee’s Wireless Facilities Attachments to Poles must not physically or electronically interfere with or obstruct any other party’s equipment on the Pole. All Wireless Facilities Attachments that are installed on Georgia Power’s Poles must be clearly labeled as to the owner of such Wireless Facilities. Licensee shall periodically inspect its Wireless Facilities and replace the labels if necessary to insure that the Wireless Facilities remain clearly labeled.
5. A. If Licensee is using Wireless Facilities Attachments for the provision of telecommunications service as defined in 47 U.S.C. 153(53), Licensee must obtain any necessary FCC license and provide Georgia Power with written proof of such license. If no FCC license is required, the Licensee must provide a written statement to Georgia Power warranting that all

Wireless Facilities Attachments are exempt from a license requirement and that all such Wireless Facilities comply with the FCC's equipment authorization requirements contained in Part 15 of the FCC's rules.

B. Licensee is solely responsible for ensuring that the radio frequency ("rf") radiation emitted by its Wireless Facilities Attachments, alone and/or in combination with any and all sources of rf radiation in the vicinity, is within the limits permitted under all applicable governmental and industry standard safety codes, including without limitation, those set forth in 47 C.F.R. § 1.1310 of the FCC rules, as applied in circumstances where there is uncontrolled access to the rf emitting facilities and the FCC's *OET Bulletin 65*. Licensee acknowledges and agrees that this is the appropriate standard due to the need for access to the poles by workers for Georgia Power and other attaching entities and the location of Poles, often in residential and other populated areas. Whether or not the FCC would require a formal environmental assessment, with respect to each particular Wireless Facilities Attachment, Licensee shall complete in writing and retain in Licensee's records the analysis specified in 47 C.F.R. § 1.1310 of the FCC's rules and the FCC's *OET Bulletin 65* for each and every one of its Wireless Facilities Attachments and only proceed if the permissible limits stated therein are not exceeded. Licensee shall provide Georgia Power with the results of its analyses substantially in the form attached hereto as Exhibit B, together with the written certification, substantially in the form attached hereto as Exhibit C, of a professional engineer, acceptable to Georgia Power, that each proposed installation of Wireless Facilities meets the standard for permissible rf emissions. The installation shall be re-evaluated and re-certified using the same approval process as for an initial attachment whenever Licensee proposes to replace or modify any installed Wireless Facilities. Licensee agrees to install appropriate signage on the Pole(s) to which Wireless Facilities have been attached, to warn line workers or the general public of the presence of rf radiation and the need for precautionary measures. Licensee shall periodically inspect the signage and replace the signage if necessary to insure that the signage, including text and warning symbols, remains clearly visible. This provision shall not apply to wireless camera Attachments provided that the wireless camera Attachment has a maximum output power of less than 5.6 Watts and (i) Licensee submits to Georgia Power the manufacturer specifications of said wireless camera Attachment and (ii) Licensee certifies in writing to Georgia Power that the maximum output power of said wireless camera Attachment is less than 5.6 Watts and the antenna conforms to manufacturer's specification.

C. Due to the need for access to the poles by line workers and contractors for Georgia Power and other attaching entities, Licensee will install on each pole to which it has attached a transmitting antenna a switch that operates to disconnect power from the antenna. In ordinary circumstances, line workers and contractors will call Licensee's Network Operations Center ("NOC") to inform the NOC of the need for a temporary power shut-down. Licensee will power down its antenna remotely upon the call to its NOC provided that, for scheduled work, the power-down will occur during normal business hours and with 24 hours advance notice. After remote power-down by Licensee, workers or contractors for Georgia Power or other attaching entities may also operate the power disconnect switch in order to ensure that the antenna is not remotely re-energized while work on the pole is still in progress. In circumstances involving an imminent threat to the safety of life or property, the power-down will be with such advance notice as may be practicable. If circumstances warrant, workers and contractors for Georgia Power and other attaching entities may accomplish the power-down by operation of the power disconnect switch without advance notice to Licensee. In all instances, the Licensee's NOC will

be informed when the work has been completed and Georgia Power or the contractor shall confirm that the power disconnect switch is in the connected position.

D. Licensee acknowledges and agrees that it is solely responsible for ensuring compliance with any and all FCC antenna registration requirements, Federal Aviation Administration (“FAA”) air hazard requirements, or similar requirements with respect to the location of Licensee’s Wireless Facilities on Georgia Power’s Poles. Without limitation, Licensee acknowledges and agrees that Georgia Power’s Poles are not “antenna support structures” within the meaning of the FCC’s rules and that, accordingly, Georgia Power has no obligation of its own in this regard to register the Poles with the FCC, the FAA, or other agency.

E. Licensee further acknowledges and agrees that Georgia Power is under no obligation, either with respect to its own facilities or those of any other attaching entity, as to facilities now existing or in the future, to protect against harmful interference to the rf signals of the Licensee, as may emanate or arise from any source.

**LICENSEE:**

*Adopted at \_\_\_\_\_, Georgia this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.*

**CITY OF PEACHTREE CORNERS**

By: \_\_\_\_\_  
Mayor  
City of Peachtree Corners

Attest:

\_\_\_\_\_  
Name & Title

Entered upon the Minutes, Minute Book \_\_\_\_\_, Page \_\_\_\_\_.

Date: \_\_\_\_\_

**GPC:**

**GEORGIA POWER COMPANY**

By: \_\_\_\_\_ (SEAL)  
Leslie R. Sibert, Distribution Vice President

Date: \_\_\_\_\_

# **Exhibit A**

## **Request for Permission to Attach Wireless Facilities**

## **Exhibit B**

### **Analysis of the Radio Frequency Emissions of Proposed Wireless Facilities Under Section 1.1310 of the FCC's Rules and the FCC *OET Bulletin 65***

## Exhibit C

### Radio Frequency Emissions Certification

The Effective Isotropic Radiated Power (“EIRP”) of the Wireless Facilities shall comply with Part 15 of the FCC Rules and levels of radio frequency exposure shall comply with Section 1.1310 of the FCC’s Rules and with the FCC’s *OET Bulletin 65*, latest revisions.

Will the Wireless Facilities that are the subject of the accompanying Request for Permission to Attach Wireless Facilities, dated \_\_\_\_\_, as installed, comply fully with the radio frequency exposure limitations at all distances for General Population/Uncontrolled Environments as specified by the Federal Communications Commission at 47 C.F.R. §1.1310 (or its successor regulation), the FCC’s *OET Bulletin 65*, latest revision, and any applicable state radio frequency exposure standards?

\_\_\_\_ Yes

\_\_\_\_ No

#### Certification:

I certify that: (i) I am a registered professional engineer with experience regarding radio frequency emissions; (ii) I have performed the analysis specified in 47 C.F.R. § 1.1310 of the FCC’s rules and *OET Bulletin 65* for each and every one of the Wireless Facilities Attachments covered in the Request for Permission to Attach Wireless Facilities, dated \_\_\_\_\_, and (iii) the answer given above is true.

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Print Name

\_\_\_\_\_  
License Number

**Georgia Power Company  
Governmental Pole  
Attachment License  
Agreement**

Between  
Georgia Power Company  
and  
City of Peachtree Corners

Addendum for  
Attachment of Banners and Seasonal Decorations

WHEREAS, Georgia Power Company (hereinafter “Georgia Power”) and City of Peachtree Corners (hereinafter “Licensee”) have entered into a Pole Attachment License Agreement, dated \_\_\_\_\_ (hereinafter the “Agreement”); and

WHEREAS, Georgia Power and Licensee mutually desire to expand the scope of the Agreement to allow for the Attachment of Banners and Seasonal Decorations to the Poles of Georgia Power;

NOW THEREFORE, in consideration of mutual promises contained herein, Georgia Power and Licensee agree as follows:

1. The terms and conditions of the Agreement shall govern the Attachment of Banners and Seasonal Decoration to the Poles of Georgia Power, except as explicitly stated in this Addendum.
2. “Banners” shall mean a sign that is not made of rigid material nor enclosed in a rigid frame so as to allow movement, and that is mounted to a Pole, along with all the equipment required to accommodate the installation of said sign. All Banners must comply with Georgia Power’s specifications.
3. “Poles” shall include, for the purposes of this Addendum for the Attachment of Banners and Seasonal Decorations to the Poles of Georgia Power (the “Addendum”) only, Georgia Power distribution poles as well as Georgia Power lighting poles and other similar structures.
4. “Seasonal Decorations” include natural or artificial evergreens, ornaments, lanterns, artificial candles, figurines and the like, which may be illuminated or otherwise electrified, along with all the equipment required to accommodate the installation of said items. Seasonal Decorations do not include any form of wireline or wireless communications device, signs, commercial advertising or messages, political advertising or messages, pole-to-pole banners or fixtures, street-crossing banners or fixtures, open flames of any type, or any other fixture of whatever type, size or character that Georgia Power, in its sole discretion, shall prohibit or refuse to allow to be placed on its Poles. All Seasonal Decorations must comply with Georgia Power’s specifications.
5. Licensee may not affix Banners or Seasonal Decoration to any Pole until Licensee requests and receives written permission from Georgia Power, using the form attached hereto as Exhibit A, “Banner and Seasonal Decoration Permit Application.” Georgia Power may deny any such request in its sole discretion, and nothing in this Addendum shall be construed to require Georgia Power to approve any request under any circumstances. Such request(s) for permission to attach Banners and Seasonal Decorations shall include information as to the number of Poles Licensee plans to attach to, a description of the Banner or Seasonal Decoration to be attached, and the duration of the Banner or Seasonal Decoration display. All such requests shall be submitted to:

Georgia Power Company  
Attn: Joint Use Coordinator  
241 Ralph McGill Boulevard, N. E.  
Bin 10140

Atlanta, GA 30308-3374  
Email: jdwilson@southernco.com  
Facsimile: 404-506-2925

6. Licensee's Banners and Seasonal Decorations must be located in a space designated by Georgia Power. The Banners and Seasonal Decorations must comply with: all Legal Requirements, including, but not limited to, separation and clearance requirements and all Georgia Power construction standards and requirements. Licensee's Banners and Seasonal Decorations must not interfere with or obstruct any other party's equipment on the Pole.

7. Licensee shall remove all of its Banners and Seasonal Decorations from Georgia Power's Poles no later than 10 days after the date specified in Licensee's Banner and Seasonal Decoration Permit Application. Licensee shall give written notice to Georgia Power upon removal of any Banners or Seasonal Decorations. If Licensee does not immediately so remove all such Banners and Seasonal Decorations, then Georgia Power shall be entitled to remove any remaining Banners or Seasonal Decorations at Licensee's sole cost and expense and without any liability therefor. Georgia Power may, in its discretion, arrange for Licensee to leave on Georgia Power's Poles some or all equipment necessary to accommodate the installation of any Banner or Seasonal Attachment removed under this Section 7.

8. In the event that any of Licensee's Seasonal Decorations require electric power, Georgia Power's written approval shall describe the manner in which such Seasonal Decorations must be connected to sources of power on Georgia Power's Distribution Pole(s). The Seasonal Decorations that require electric power shall be connected to Georgia Power's electric power distribution system present on the Pole on which the Seasonal Decoration is located and shall not draw on any other power source. Electric power to Licensee's Seasonal Decorations shall be billed based on an estimated energy use of the Seasonal Decoration for the duration of the display. Georgia Power shall invoice Licensee for the estimated amount of energy used, and Licensee agrees to pay Georgia Power for such power within a reasonable time of receipt of said invoice.

*(Signatures Commence on Next Page)*

AGREED TO AND EFFECTIVE, this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_,  
by:

**LICENSEE:**

*Adopted at \_\_\_\_\_, Georgia this \_\_\_\_\_ day of \_\_\_\_\_, \_\_\_\_\_.*

CITY OF PEACHTREE CORNERS

By: \_\_\_\_\_

Mayor  
City of Peachtree Corners

Attest:

\_\_\_\_\_  
Name & Title

Entered upon the Minutes, Minute Book \_\_\_\_\_, Page \_\_\_\_\_.

Date: \_\_\_\_\_

**GPC:**

**GEORGIA POWER COMPANY**

By: \_\_\_\_\_ (SEAL)

Leslie R. Sibert, Distribution Vice President

Date: \_\_\_\_\_

**EXHIBIT A**  
**Banner and Seasonal Decoration Permit Application**

---

Name of Licensee

---

Mailing Address

---

City

State

Zip Code

Number of Poles to Attach Banners or Seasonal Decorations: \_\_\_\_\_.

Location of Poles (sketch attached hereto as Exhibit "B"):

---

---

Description of Displayer's Banners or Seasonal Decorations (visual description attached hereto as Exhibit "C"): \_\_\_\_\_

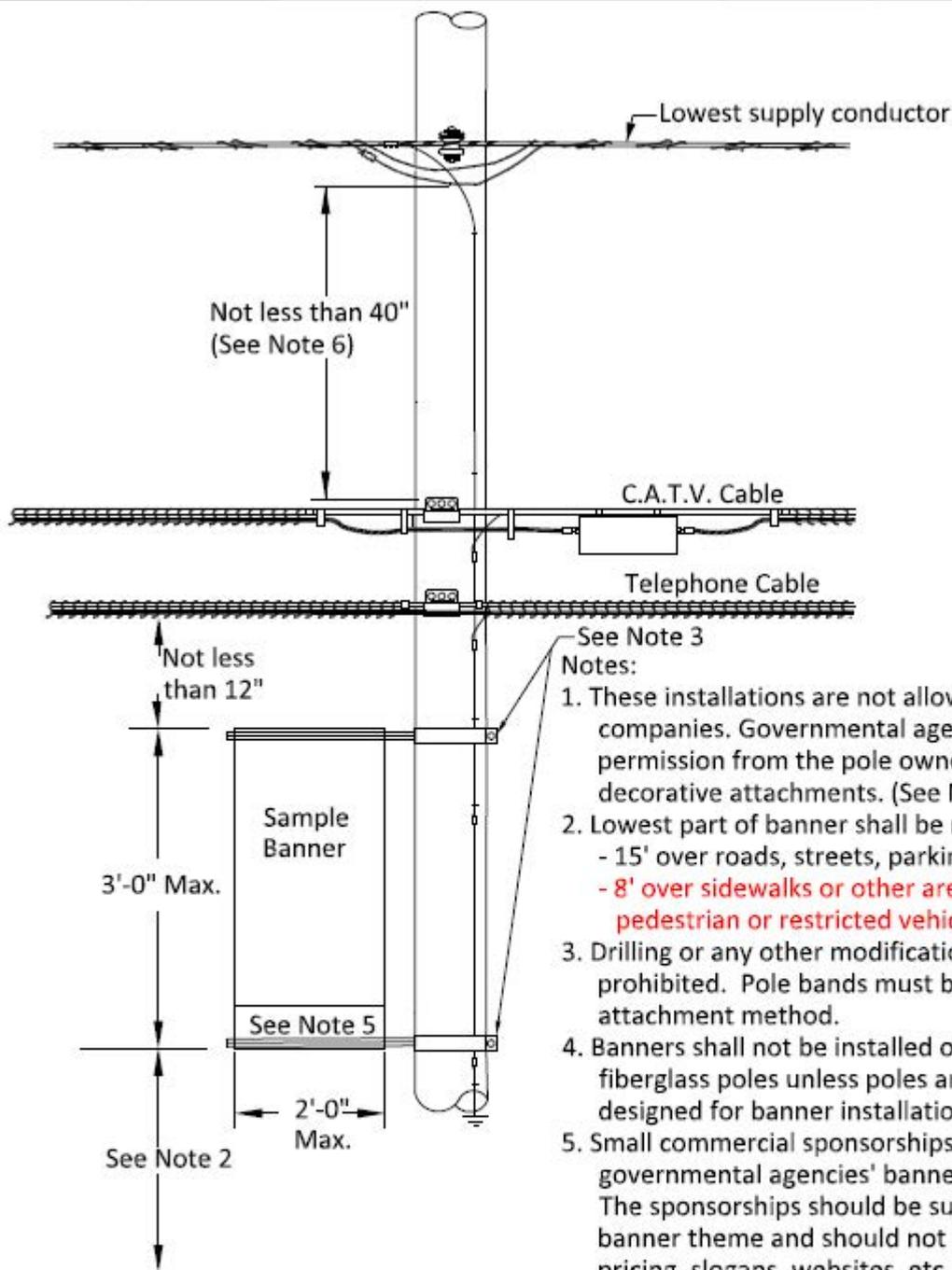
---

---

Duration of Display: \_\_\_\_\_

**EXHIBIT B**  
**Pole Description**

**EXHIBIT C**  
**Visual Banner or Seasonal Decoration Description**



See Note 3

Notes:

1. These installations are not allowed at all operating companies. Governmental agency must obtain permission from the pole owner before making decorative attachments. (See NESC 217A5)
2. Lowest part of banner shall be not less than:
  - 15' over roads, streets, parking lots, or alleys.
  - 8' over sidewalks or other areas subject only to pedestrian or restricted vehicle traffic.
3. Drilling or any other modification to pole is prohibited. Pole bands must be utilized as the attachment method.
4. Banners shall not be installed on decorative or fiberglass poles unless poles are specifically designed for banner installation.
5. Small commercial sponsorships located on governmental agencies' banners are permissible. The sponsorships should be subordinate to the banner theme and should not include advertising, pricing, slogans, websites, etc. All banners are subject to review and approval by pole owner.
6. All attachments shall be not less than 40" below the lowest supply conductor

SUBJECT **OVERHEAD DISTRIBUTION**

DETAIL **JOINT USE - TYPICAL SINGLE POLE BANNER ATTACHMENT INSTALLATION**

Date **12-30-11**

REVISED **08-22-13**



Small text or logo in the bottom right corner of the page.



**Work Session**  
**Mechanicsville**  
**School**  
**D. Wheeler**



# Memo

---

TO: Mayor and Council

CC: Julian Jackson, City Manager

FROM: Diana Wheeler, Community Development Director

DATE: April 19, 2016

SUBJECT: Mechanicsville School

---

Staff evaluated the Mechanicsville School building to determine its condition and assess basic requirements to make it functional for routine use. In addition, the Building Official prepared an estimate for site work that would need to be done in order to accommodate the building. The estimate for all the work is \$140,000 and the detail of the work to be done is attached.

# Mechanicsville School

	PROJECT TASKS	ESTIMATED COSTS
PERMITS	Grading Permit	\$0.00
	Moving Permit	\$0.00
	Foundation Permit	\$0.00
	<b>Subtotal</b>	<b>\$0.00</b>

PROJECT DEVELOPMENT	Site Development	\$30,000.00
	Paved Parking w/ADA Spaces	\$30,000.00
	Footings and Foundation	\$20,000.00
	New Sewer Lines and Tap	\$5,000.00
	New Electrical Service	\$5,000.00
	<b>Subtotal</b>	<b>\$90,000.00</b>

PROJECT POST DELIVERY	Install ADA Compliant Ramp	\$3,000.00
	Replace Roof	\$10,000.00
	Install New HVAC	\$8,000.00
	Install Insulation Throughout	\$10,000.00
	Floor Repairs	\$2,000.00
	Interior Plumbing to ADA Standards	\$4,000.00
	Interior Electrical	\$4,000.00
	Exterior Siding and Trim Repair	\$4,000.00
	Repainting of Exterior and Interior	\$5,000.00
<b>Subtotal</b>	<b>\$50,000.00</b>	

MISC. MGMT	Annual Termite Service Contract	\$500.00
	Annual Bldg/Grounds Maint. Contract	\$3,600.00
	<b>Subtotal</b>	<b>\$4,100.00</b>

<b>Subtotals</b>	
Project Completion	<b>\$140,000.00</b>
Maintenance (Annual)	<b>\$4,100.00</b>

## Conclusion

Site work and paved parking with ADA compliant spaces will be a large portion of the costs.

Footings will need to be in place prior to moving the structure to its new location.

Upon moving the structure, the foundation can then be completed.

Additional damages may be caused during the move and result in the need for additional repairs.

A new roof is recommended to account for this move and the possibility of roof damage.

New sewer lines and a new sewer tap will need to be installed.

A new 200 amp electrical service will need to be installed

The structure needs to be completely insulated and a new HVAC system installed.

The floor is in need of repairs and may need additional repairs upon moving.

Areas of exterior siding and exterior trim are in need of repair and/or replacement after move.

To be in compliance with ADA standards for public use, some new plumbing work is required.

In addition, to be compliant with current electrical requirements and to eliminate some potential hazards (i.e. GFCI protection, exit signage, emergency lighting, etc.), some new electrical work will be required.

Upon completing the building setup a new ADA compliant ramp will need to be installed.

Upon repair and/or replacement of damaged exterior siding and trim, repainting is needed.

**Note**

The estimated costs do not show costs of utilities other than the initial setup costs.